

# Ocean Grove Town Centre

## Urban Design Framework

June 2014



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## Urban Design Framework

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*June 2014*

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*Council has made every effort to ensure the accuracy of the material and integrity of the analysis presented, although no liability is accepted for any actions taken by others on the basis of the contents of this report.*

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# Introduction

## What is an Urban Design Framework and why do we need one?

An Urban Design Framework (UDF) is a plan that describes and illustrates how planning and design policies and principles should be implemented in an area where there is a need to manage, guide and promote change.

Ocean Grove is growing and it is vital that the future Town Centre continues to thrive. This plan will unlock its potential and ready the Town Centre for the growth that will occur over the next 10 years.

This strategy addresses:

- Movement within and around the Town Centre;
- Provision of space for businesses, services, commercial and community facilities needed as the community grows; and
- How to preserve and build on Ocean Grove's character and identity.

The UDF provides:

- A vision and design principles for how the future Town Centre could develop to make the most of the change and new investment that will come with the forecast population growth.
- Assists in the assessment of future development proposals to ensure new developments are consistent with the coastal character of Ocean Grove.
- Delivers a road map for how infrastructure investment in Ocean Centre should occur in coming years.



Figure 1: Location of Ocean Grove

## Site Context

Ocean Grove is located on the southern Bellarine, approximately 25kms from Geelong. It is bounded by the surf of the Southern Ocean, the Barwon River to the west and native bush land to the north. The primary entry to the town is from Geelong via Grubb Road/Tuckfield Street or Wallington Road. The Barwon Heads - Ocean Grove Road also provides a key connection from the west. Presidents Avenue feeds local traffic into the Town Centre and provides an important north-south link to Thacker Street and Shell Road. Figure 1 shows the location of the town in relation to Geelong and The Bellarine.

Ocean Grove is a designated growth location on The Bellarine. The population is expected to grow from 12,555 to 2011 to 15,000 - 21,000 by 2020<sup>1</sup>. Growth is expected in both the permanent resident population and holiday population.

The majority of this growth will be accommodated in the growth corridor between Grubb Road and Banks Road on the north east side of the town but also through infill housing in the existing residential areas.

To meet the growing needs of the new and existing residents, particularly families moving into Ocean Grove, Council has invested significant capital resources over the past 4 - 5 years into the development of:

- Bellarine Aquatic and Sports Centre opened in 2008 at a cost of \$8 million;
- Ocean Grove Integrated Children's Centre to be opened in early 2014 at a cost of \$6 million; and
- New football, netball and soccer facilities at Shell Road.

Ocean Grove is a popular holiday destination with many families having long associations with camping and visiting the region annually. The current peak holiday population of Ocean Grove is estimated at 30,000<sup>2</sup>.

1 Ocean Grove Structure Plan (2007) based on annual growth of 2% - 4%. Current annual growth in 2.2%.

2 Peak holiday period - such as New Years Eve - City of Greater Geelong Economic Indicators Bulletin 2012

# Study Area

## Study Area

For the purposes of the UDF the Town Centre is generally defined as:

- South of Powell Street;
- Properties on the east and west of Presidents Avenue;
- North of Dare Street; and
- West of Eggleston Street.

This is the primary study area and is shown in Figure 2.

There is a broader context to the Town Centre that influences how the centre functions. Connections to and from the centre have been considered as part of the development of the UDF particularly the existing and future arterial road corridor on Orton Street, The Parade and The Terrace.



Figure 2: Aerial photo of study area

# Site Context

## Role of Ocean Grove Town Centre

The Ocean Grove Town Centre is a vibrant and thriving shopping strip providing a dual role as a destination for visitors and a key service and retail centre for local residents. The centre also provides shopping and services for nearby smaller townships of Wallington, Barwon Heads, Point Lonsdale and Queenscliff.

The Town Centre has developed along The Terrace and extended north to The Parade and The Avenue. The Town Centre consists of 13,000 sqm of retail floor space and 3,600 sqm of office floor space.

The Centre provides the daily shopping needs for the western part of the township and includes a Coles supermarket, restaurants, offices, speciality retail, hardware, medical services, banks, real estate agents, police and community services and facilities.

## Policy context

The recently adopted G21 Regional Growth Plan is one of eight being developed across Victoria to respond to the challenges of growth. With a time frame until 2050, the Regional Growth Plan aims to grow the broader G21 region to 500,000 people. Ocean Grove is a key growth node in this plan.

The **Structure Plan** and **Clause 21.14 - Bellarine Peninsula** of the Greater Geelong Planning Scheme identify the following objectives for Ocean Grove, in part:

- Encourage development which respects the coastal landscape setting of Ocean Grove, by:
  - Providing reasonable sharing of views of the coast and foreshore
  - Promoting contemporary design that reflects the existing scale, setbacks, spacing, forms and materials of the buildings in the locality.
- Encourage a range of accommodation and housing options, including aged care within and adjacent to the Town Centre and other existing and proposed activity centres.
- Ensure the Town Centre remains the primary retail centre for Ocean Grove by providing for a range of retail, business and accommodation uses.
- Support expansion of the Town Centre, particularly in a northerly direction up to The Avenue.
- Support development of a new Neighbourhood Activity Centre on Grubb Road, to service the future population of the north-east growth area.

The **Ocean Grove Structure Plan (2007)** made recommendations about the need for more detailed planning in the Town Centre.

Council's **Housing Diversity Strategy (2007)** identifies the land within 400m of the Town Centre for increased medium and high density housing.

The **Greater Geelong Retail Strategy (2006)** and **Clause 21.07 Economic Development and Employment** establishes a retail hierarchy within the municipality to provide for a viable and accessible retailing sector. The strategy nominates the commercial area of Ocean Grove as a 'town centre' and the existing Shell Road Market Place Centre as a 'neighbourhood centre'.

The Shell Road Market Place has a current floor space of 5,200 sqm and includes a supermarket, speciality shops and fast food outlets. The shopping centre has the ability to expand to 7,500 sqm with a small expansion to the north and rearrangement of the site.

The Grubb Road growth area known as the Oakdene and Kingston Downs Estates, will provide approximately 3,200 additional lots. A future neighbourhood centre is nominated in this growth area fronting Grubb Road. 8ha of land has been rezoned to Business 1 and the Structure Plan envisages a centre of up to 7,000sqm.

# Developing the UDF

Development of the UDF has been underpinned by the input Council received from residents, business, community associations, sporting clubs, schools and government agencies through an Enquiry by Design (EbD) workshop.

Over 90 delegates gathered for a two day conference at the Ocean Grove Surf Life Saving Club to uncover the community aspirations for the Town Centre.

Council and the community were assisted by a Design Team from TTM Consulting (traffic engineering), Design Urban (urban design), Urbacity (retail and activity centre planning) and groupwork (facilitation).

## What we heard the future Ocean Grove Town Centre should be

These objectives are the culmination of the work on day one and represent the **preferred future outcomes** for Ocean Grove expressed by the community:

- *Ocean Grove has 'heart and soul' with spaces for the community to gather. It is attractive and vibrant with retail, business, social and cultural activities. It's big beach, ocean and surf character is evident.*
- *The urban structure encourages people living in quality housing and working in the Town Centre.*
- *Our Design and Planning Guidelines support developments that advance the special character of Ocean Grove.*
- *Ocean Grove has a strong connection to and respect for its many environments. Conservation, new plantings and 'green' links also add to the area's special character.*
- *Ocean Grove provides a network of roads that don't always give priority to traffic. There are strong pedestrian and cycle connections between the Park, Town Centre and Beach.*
- *Ocean Grove intersections and crossings are safe.*
- *Ocean Grove has a well-structured car-parking regime. There's enough parking spaces, access and circulation is easy and it's well managed.*
- *Ocean Grove has a road network that provides for local traffic and servicing. Through-traffic is slowed as it moves around the town.*

The Ocean Grove community want Ocean Grove to be seen by others as:

- Unspoilt, Beautiful Environment
- Friendly, Welcoming and Hospitable
- Relaxed and Peaceful
- Vibrant and Attractive
- Safe

Part of the consultation was the development of a Concept Masterplan. A copy of the workshop outcomes is provided as Appendix 1:



Figure 3: Concept masterplan from EbD

The preferred future outcomes and Concept Masterplan have formed the basis for the development of the UDF. The implementation of the community objectives are explored in detail in the following chapters of the UDF:

## Chapter 2: Urban Design Framework

## Chapter 3: Moving Safely

## Chapter 4: A Vibrant Town Heart

## Chapter 5: Managing Parking



Figure 4: Enquiry by Design workshop underway



# Urban Design Framework

*Ocean Grove is growing and it is vital that the Town Centre continues to thrive into the future. This plan will unlock its potential and ready the Town Centre for the growth and change that will occur over the next 10 years.*



# Urban Design Framework

The guiding design principles of the Urban Design Framework are:




















- Enhance the quality of the experience for locals and visitors in the Town Centre by improving the public realm
- Activate the perimeter block of the Town Centre with shops
- Create vibrant outdoor shopping and eating areas
- Promote economic diversity in the Town Centre
- Reinforce Ocean Grove Town Centre as the primary retail centre for Ocean Grove in the face of new competition at Shell Road and Grubb Road
- Create new meeting places and public spaces
- Prioritise pedestrian and cyclist safety and amenity in new development and streetscape upgrades
- Improve safety for drivers, cyclists and pedestrians
- Create connections from the town centre to the beach and Ocean Grove Park
- New development meets its car parking demand or provides additional spaces within the Town Centre

The primary recommendations and projects identified in the Urban Design Framework are:

- Consolidate commercial land use within the existing zoned land to promote a compact Town Centre
- Create a town square in The Terrace
- Signalise the intersection of Orton Street and Presidents Avenue
- Promote high quality mixed-use development of three and four storeys that respects the coastal setting of Ocean Grove
- Upgrade streetscapes in Presidents Avenue, Hodgson Street and The Avenue
- Square up the intersection of Hodgson Street and The Parade
- Preserve The Terrace, east of Hodgson Street as the long term arterial road through the Town Centre
- Improve footpath connections to the Town Centre

An overall framework plan is shown on the following page.

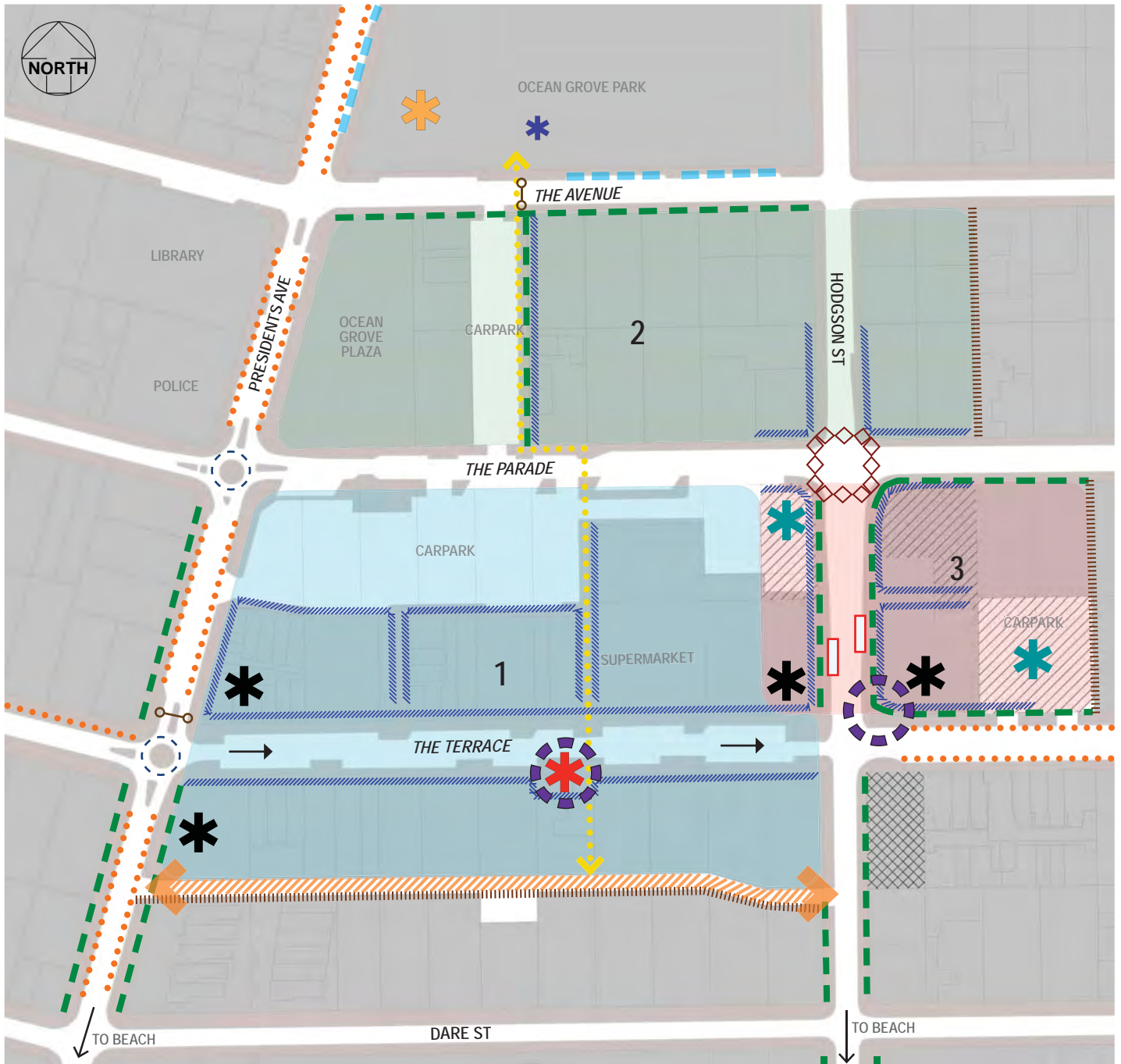
## LEGEND

-  Active frontages - shop & office
-  Signature building
-  New Town Square
-  High quality landscaping & public art
-  New bike path
-  Small roundabout
-  Improved pedestrian connection
-  Proposed bus stop
-  'Squared-Up' intersection
-  New pedestrian crossing
-  Meeting point
-  Rear access and loading
-  Development sites
-  Indent existing car parking
-  Cenotaph relocated from The Parade
-  New performance space
-  Retain for future road widening
-  Residential interface
-  Off-set car parking within proximity to Town Centre or replace on-site

## PRECINCTS

- 1. The Terrace Retail and Residential Precinct**
  - Generally 3-4 storeys
  - 4 storey in The Terrace limited to south side of the street
  - New development to address the street
  - Zero front setbacks
  - Upper level development to be set back and generally recessive
  - Maintain the human scale of the street
- 2. The Avenue Office and Residential Precinct**
  - Generally 3 storeys
  - Upper level development to be set back and generally recessive
  - New development to address the street and nearby Ocean Grove Park
  - Strong landscape character and connection to Ocean Grove Park
- 3. Hodgson Street East Mixed Use Precinct**
  - Generally 3-4 storeys
  - Upper level development to be set back and generally recessive
  - New development to address the street and nearby residential area
  - Minimal carparking visible from the street

# THE FRAMEWORK PLAN





# Moving Safety

## Preferred future outcomes

- *Ocean Grove provides a network of roads that don't always give priority to vehicular traffic. There are strong and safe pedestrian and cycle connections between the Park, Town Centre and Beach.*
- *Ocean Grove has a road network that provides for local traffic and servicing. Through-traffic is slowed as it moves around the town.*



# Moving Safely

## Opportunities and Constraints

A Smart Roads Network for Ocean Grove Town Centre has been developed working with the Department of Transport, Planning & Local Infrastructure, VicRoads and Public Transport Victoria. The Smart Roads Network considers all road user groups (pedestrians, cycles, freight and cars) and assigns the priority of each to the various routes within the Town Centre. Smart Roads allows for a definition of a road hierarchy and assists with the detection of conflicts between user groups that should be resolved. Smart Roads is a useful tool for assessing how well infrastructure projects meet the objectives of the road hierarchy.

### Roads

The arterial road network is a significant barrier to pedestrian amenity and safety. At busy times the traffic volumes are above the level where the existing infrastructure can deliver outcomes that are town centre friendly for both drivers and pedestrians.

Surveys show that most traffic on the Barwon Heads - Ocean Grove Road is local traffic accessing the Town Centre rather than through traffic heading on to Queenscliff or Drysdale. Based on this, there is opportunity to slow through traffic and improve local circulation within the Town Centre.

The intersection of Orton Street and Presidents Avenue was overwhelmingly the key concern identified by the community due to the volume of traffic, poor sight lines and a lack of pedestrian crossing facilities.

### One-way system

During the development of the UDF a number of options were tested for changing the direction of the one-way system in The Terrace and introducing a one-way system in The Parade.

### The Terrace

The current operation of The Terrace was generally agreed to work well and there was no prevailing view within the community of the benefits of changing the direction or returning to a two way operation. Option testing for changing the direction of the one-way flow on The Terrace identified a number of challenges that make implementation undesirable. These issues include:

- Forcing drivers to make multiple right hand turns to move around the Town Centre (right turns from Hodgson St to The Terrace, from The Terrace to The Parade, from The Parade to Hodgson), which would generate a higher number of conflicts at intersections which is undesirable from both a circulation and safety perspective.
- The need for a right turn lane for traffic entering The Terrace from the Hodgson St / The Terrace

traffic signals. Due to the existing curved alignment of Hodgson St, introducing a right turn lane in this location is not desirable from a traffic safety perspective. Introducing the right turn lane would also complicate longer term infrastructure design by reducing the amount of space available in an already constrained area.

- Increased movements directly to The Terrace from Hodgson Street. This would make parking in The Terrace the first point of call for parking in the Town Centre, and would generate additional vehicle movements and congestion in The Terrace during the peak season.
- The works required for this option, including civil works on Hodgson St and The Terrace, in addition to signage, line-marking, and traffic signal infrastructure would generate a significant capital cost for implementation.

The direction of travel on The Terrace should be reviewed when The Terrace becomes the arterial road as this will be a substantial change to how users access the Town Centre.

### The Parade

Incorporating a one-way system in The Parade would generate confusion for visitors and would make circulation around the Town Centre problematic. By incorporating a one-way system the design would force traffic coming from the west and vehicles going to the east into local streets of The Avenue or The Terrace. From a circulation perspective this is undesirable and would create a more complicated operation of the car park and increased traffic nearby.



Figure 4: One-way traffic flow on The Terrace

# Moving Safely

## Active transport

Whilst Ocean Grove has a beautiful landscape and setting that should make it a good place for walking, the town has an extremely poor pedestrian network and low amenity for pedestrians, even within the heart of the Town Centre.

The dominance of traffic on Hodgson Street undermines the success of the retail development east of the Town Centre.

Identified key pedestrian conflict points and priority routes are shown in Figure 5.

Cyclists are equally disadvantaged by the dominance of private vehicles and lack of cycle infrastructure.

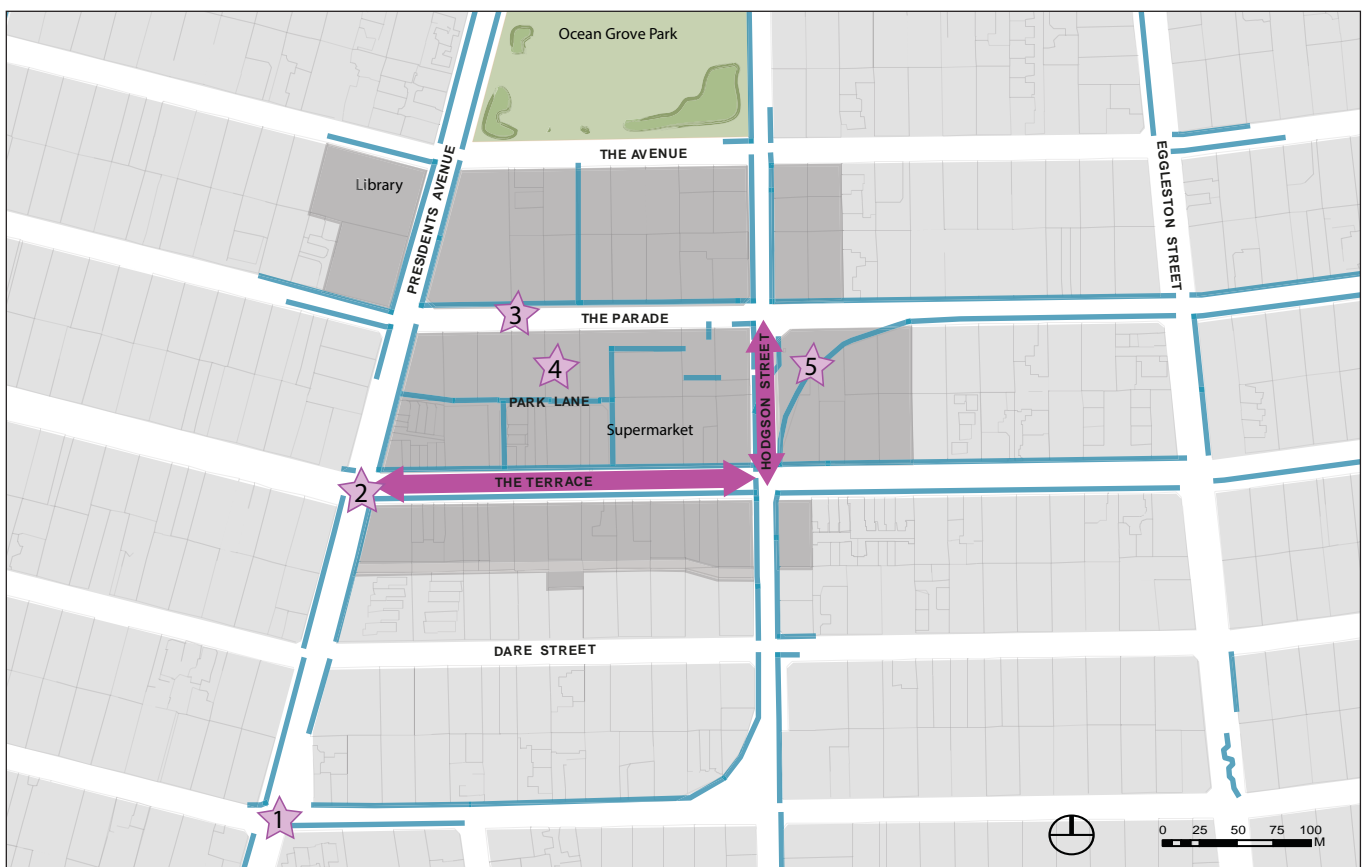


Figure 5: Pedestrian conflict points and priority areas

### Legend

- Existing footpath
- ★ Poor sight lines & high traffic volume
- ★ Poor sight lines & high traffic volume
- ★ Vehicles failing to stop at crossing points for pedestrians
- ★ No pedestrian facilities in car park due to high number of parking spaces provided
- ★ Wide road reserve and high traffic volume
- Pedestrian priority

# Moving Safely

## Public Transport

The road network is structured such that the Town Centre and surrounding residential areas should be able to be serviced by bus transport with the arterial road network acting as a transit corridor.

However, service frequencies are low and routes circuitous, with Geelong – Ocean Grove services running hourly for the majority of the day. Public transport travel time between Ocean Grove Town Centre and Geelong is approximately 1 hour and travel time to the Marshall Station is approximately 45 minutes.

The existing bus stop in Hodgson Street is shown in Figure 6 and existing and proposed bus routes are shown in Figure 7.



Figure 6: Existing Hodgson Street bus stop



Figure 7: Existing and proposed bus routes

### Legend

- Existing Bus Route
- Existing Bus Stop
- Proposed Bus Route
- Proposed Bus Stop

# Moving Safely

## Projects

### Road network upgrades

Whilst desirable in the long term, the diversion of the arterial road to the Terrace is not achievable due to the high costs associated with the connection at Tuckfield Street. Until such time as VicRoads duplicate Tuckfield Street south of Shell Road there is no imperative to spend significant Council resources to initiate construction of the connection.

Therefore a phased approach to upgrading the road network is proposed that balances community priorities and funding resources available. The proposed phasing is shown in Figure 8.

**Phase 1a:** Signalise Orton Street/Presidents Avenue & improve pedestrian and cycle link on Presidents Avenue.

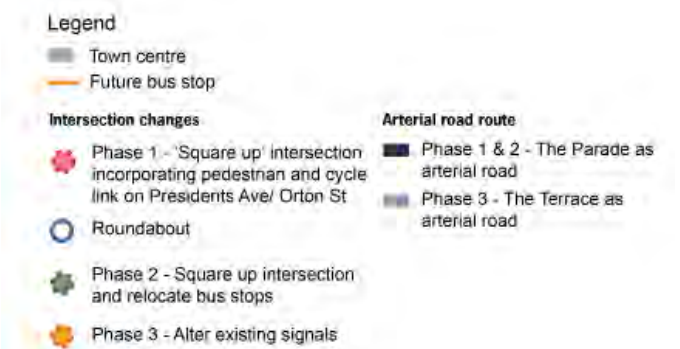
**Phase 1b:** Local treatments on Presidents Avenue.

**Phase 2:** “Square up” - Hodgson Street/The Parade Intersection.

**Phase 3:** Long term diversion of arterial road to The Terrace.



Figure 8: Proposed road network



# Moving Safely

## Phase 1a: Signalise Orton Street/Presidents Avenue Intersection

The benefits of signalising the intersection are:

- Allows creation of a safer pedestrian and cycle link to/from the Town Centre and the beach;
- Improves driver safety at known traffic blackspot;
- Creates gaps in the traffic down stream; and
- Encourages traffic to leave the Town Centre via Presidents Avenue.

In conjunction with the signalisation project, improved directional signage to the Town Centre and tree planting to establish the 'green links' can be achieved.

The primary responsibility for this project is VicRoads as Orton Street is an arterial road. Council will work with VicRoads to identify funding sources and implement supporting streetscape, roundabout and pedestrian upgrade works.

## Phase 1b: Local treatments on Presidents Avenue

As a consequence of signalisation of the intersection of Presidents Avenue and Orton Street there will be a redistribution of traffic onto Presidents Avenue, particularly traffic traveling west on The Parade and turning left onto Presidents Avenue. Two small roundabouts are recommended to improve local circulation at the intersections of Presidents Avenue and The Terrace and The Parade.

The roundabouts will provide:

- Improved circulation to and from The Parade. Vehicles coming from The Parade are currently required to make a decision to enter the car park 80-90 metres before Presidents Ave. If this decision point is missed, vehicles must navigate through the local street network (undertaking multiple left or right turns) to return to the main car park. By incorporating a roundabout in this location this problem could be avoided as a u-turn movement back into the parking area would be provided.
- Improved access and activity in The Terrace in off-peak periods. Vehicles coming from the south and west currently have to give way to vehicles coming from the north. The proposed roundabouts provide for easier access to the Town Centre from these directions. This is particularly important in the off-peak period to generate activity in The Terrace.
- Improved circulation and reduction of congestion within The Terrace. Vehicles coming from the north are currently unable to undertake a u-turn manoeuvre during times when The Terrace is congested and queued back to Presidents Ave. The proposed roundabout enables a u-turn or right turn manoeuvre,

allowing vehicles to navigate back to The Parade or surrounding on-street parking areas when looking for parking. This effectively reduces demand on The Terrace and gives drivers the option to assess the demand in The Terrace prior to getting stuck in long queues in The Terrace.

- Slower speed environment adjacent to community facilities.
- Reduced vehicle speeds and improved environment for pedestrians crossing at the existing crossings.

Figure 11 shows a concept plan for the upgrade of Presidents Avenue.



Figure 9: View south on Presidents Avenue to the intersection with Orton Street



Figure 10: Existing cycle lanes on Presidents Avenue



# Moving Safely

## Phase 2: 'Square Up' The Parade and Hodgson Street intersection

To achieve community objectives of improving the public realm and improving pedestrian amenity in the Town Centre the squaring-up of the Hodgson Street/The Parade intersection is recommended.

The benefits of squaring up the intersection are:

- Creates a new arrival point to the Town Centre;
- Provides for bus and cyclist safety;
- Results in minimal car parking loss;
- Maximises active frontages;
- A regular and permeable street network for a familiar and legible pedestrian environment;
- Co-located bus stops that deliver public transport users into the core of the Town Centre;
- Opportunity for an overall lift in the quality of the public realm in Hodgson Street;
- Improves pedestrian safety and amenity;
- Acts as a catalyst for the redevelopment of the land east of Hodgson Street that is already zoned for commercial development;
- Provides opportunity for a high quality primary retail development on both sides of Hodgson Street compared to the existing under-performing secondary frontage;
- Realises a development site that can be used to leverage funding to undertake the intersection upgrade works;
- Creates a 'green link' on Hodgson Street with street trees and landscaped central median;
- The intersection works can tie in with the long term design for the conversion of The Terrace to the arterial road;
- In the long term, can provide for on-road bicycle lanes at the intersection when the arterial road is transferred to The Terrace.

Detailed modelling of peak summer holiday traffic volumes indicate that the new intersection will operate within acceptable limits.

The squaring up of the intersection can be achieved through the signalisation or a roundabout. The use of signals offers the best pedestrian amenity whereas the roundabout offers the least delay for traffic.

Further work on the implementation of the UDF will resolve the final design of the intersection treatment.

In determining the final intersection design the following design principles should be considered:

- Maximise pedestrian and cyclist safety;
- Facilitate pedestrian crossing between The Parade main car park and Precinct 3 east of Hodgson Street;
- Minimise impact on the council car park on the north west corner of the intersection;
- Minimise acquisition of private property;
- Provide for bus stop on Hodgson Street;
- Maximising active frontages on Hodgson Street;
- Accommodate heavy vehicle movements on the arterial road and access to The Parade; and
- Fulfil the arterial road function of Hodgson Street and The Parade.

Figure 14 shows the concept design for the signalisation of intersection of Hodgson Street and The Parade providing all required vehicle movements.

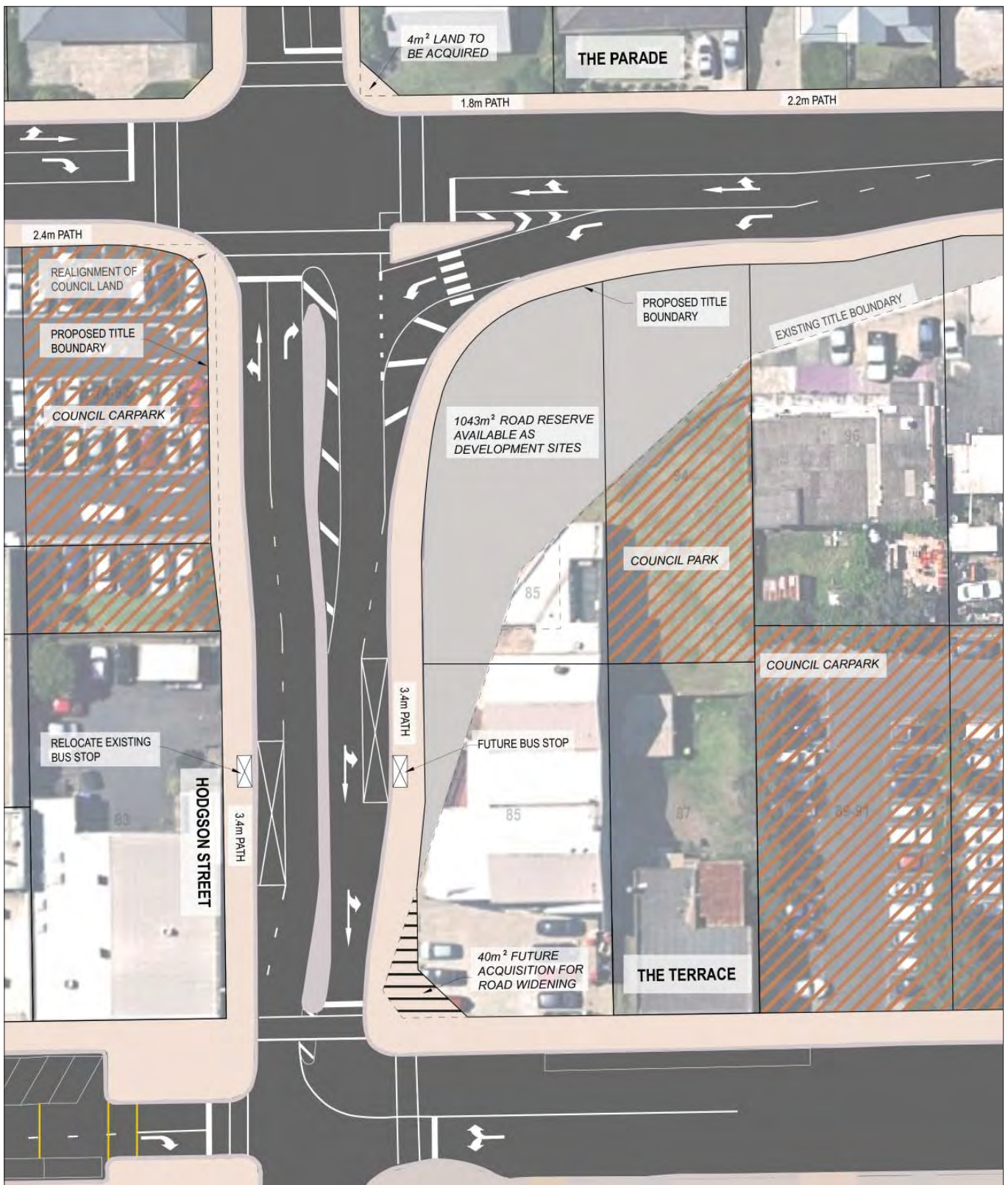


Figure 12: Existing sweeping bend on Hodgson Street - to be squared up under Phase 2 creating a new development site



Figure 13: View south of arterial road in Hodgson Street

# Moving Safely



## Legend


-  Council land suitable for redevelopment subject to off-set or replacement of carparking spaces

Figure 14: Concept design for the 'squaring up' and signalisation of Hodgson Street and The Parade intersection

# Moving Safely

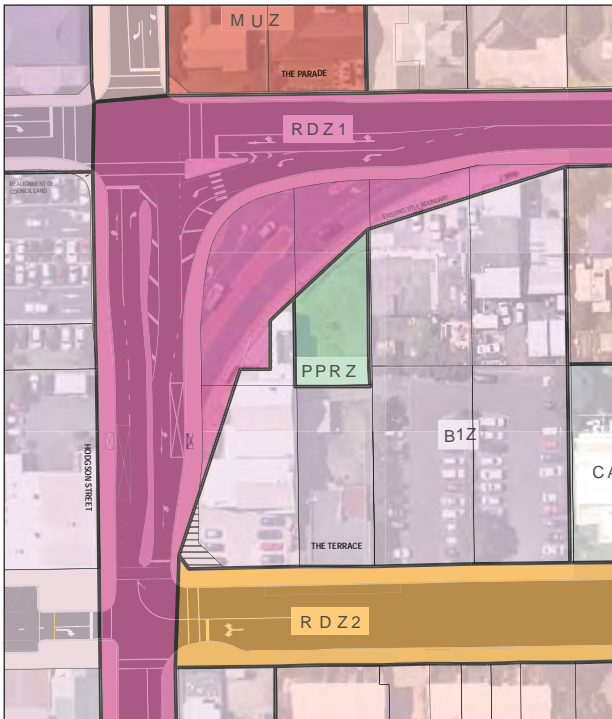


Figure 15: Existing zoning of Hodgson Street Precinct

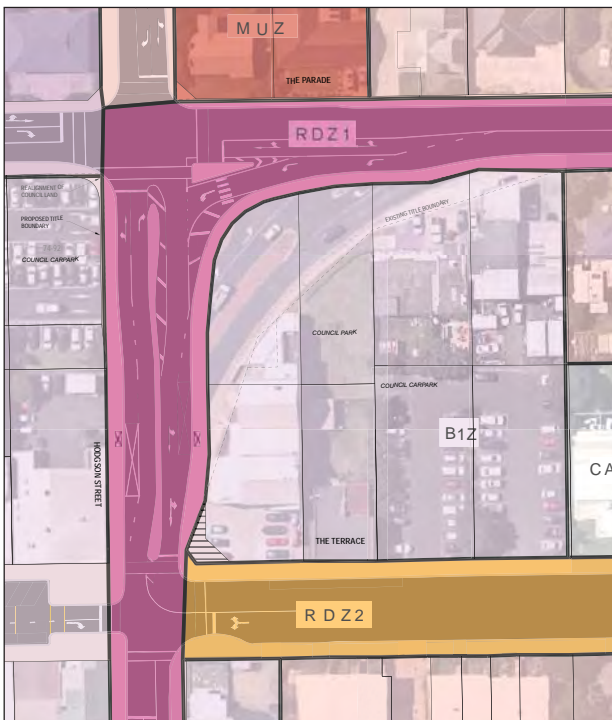


Figure 16: Indicative proposed zoning of Hodgson Street Precinct

## Legend

- Business 1 Zone (B1Z)
- Road Zone Category 1 (RDZ1)
- Road Zone Category 2
- Commonwealth Land (CA)
- Residential 1 Zone (R1Z)
- Public Park and Recreation (PPRZ)
- Mixed Use Zone

## Phase 3: Long term diversion of arterial road to The Terrace

The Terrace remains the preferred long term arterial route to divert traffic around the Town Centre.

Figure 17 provides a concept design for the future road alignment. Whilst there is no timeframe for when these works would be undertaken it is critical that planning for the Town Centre considers the future requirements. Critical to the long term plan is the retention of Council property on the south-west corner of the intersection and building setbacks and future title realignment on the north east corner described in the guidelines in the following chapter.

Council should investigate the purchase of land at 97 Dare Street and 37 Hodgson Street to provide flexibility for the future design of the intersection.

The existing road reserve in Hodgson Street, south of the intersection that has been purchased by Council, should be maintained. Whilst this land is not required to facilitate this project its intended purchase was to facilitate right turn lanes at Dare Street and this local treatment may still be required into the future.

When the arterial road is converted to The Terrace there will be the opportunity to reconsider the role and design of Hodgson Street between the The Terrace and The Parade. This could include the closure of this section of road to through traffic or retrofitting the road with bicycle lanes and extending the footpaths.

# Moving Safely

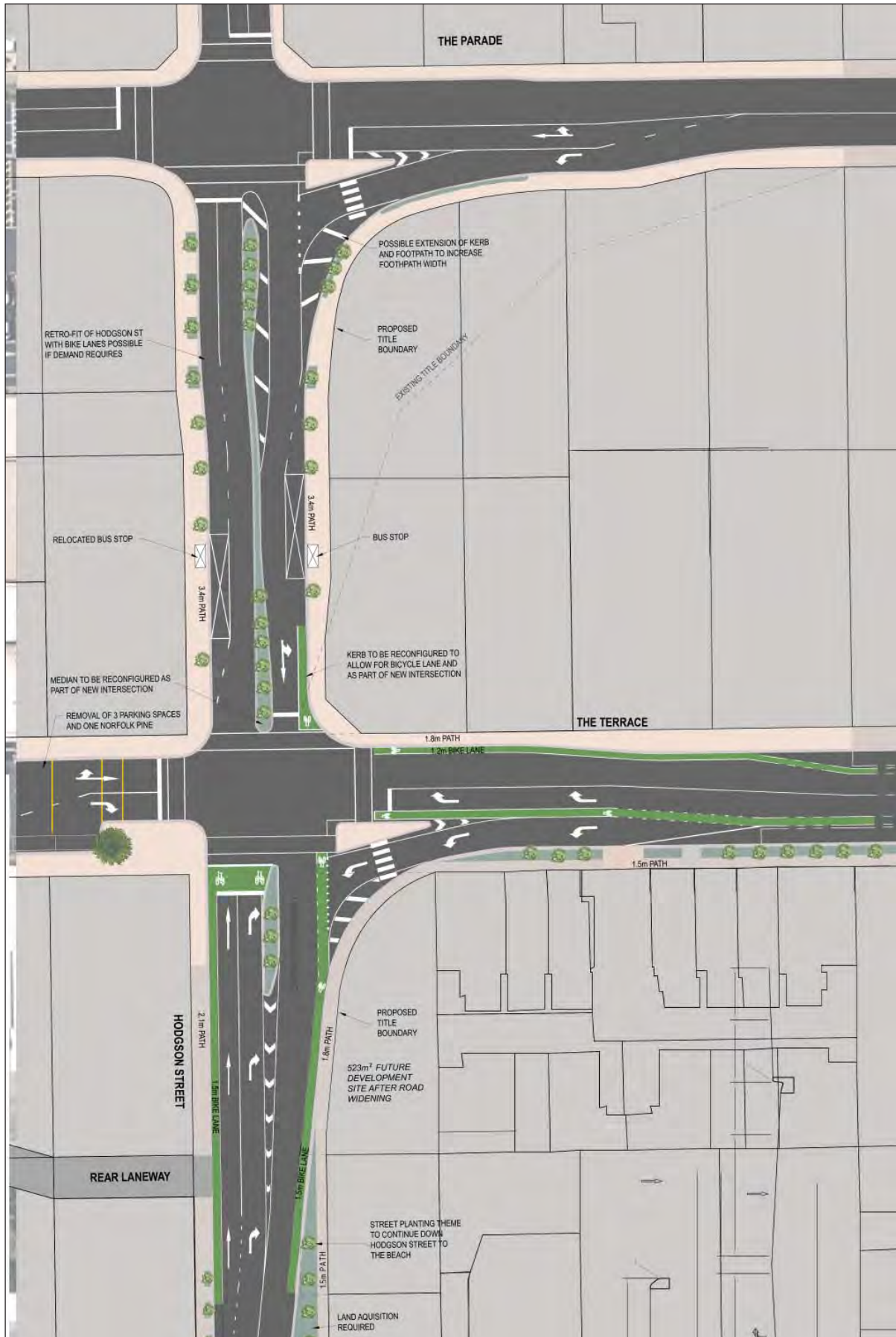


Figure 17: Concept design for the long term conversion of The Terrace to the arterial road

# Moving Safely

## Active transport network upgrades

The UDF cannot resolve all walking and cycling priorities and projects for all of Ocean Grove.

### Principle pedestrian network

The need for a strategic and coordinated approach to developing a comprehensive footpath network for all of Ocean Grove was identified through the community consultation and the development of the UDF.

It is recommended that a Principle Pedestrian Network (PPN) for Ocean Grove be developed that includes concept design, costings and an implementation strategy for the delivery of a walking and footpath network for Ocean Grove.

As part of a broader footpath strategy, installation of footpaths and shared paths should consider using materials and finishes that are in keeping with the informal streetscape character of Ocean Grove.

## Green Links

Establishing 'green links' to create a high amenity linkages between the beach and the Town Centre is at the heart of proposed pedestrian upgrades and landscape planting.

Whilst Norfolk Pines in The Terrace are integral to the Town Centre the existing constrained road reservations and overhead powerlines would prevent further successful avenues of Norfolk Pines.

There is no defining indigenous species that has the shape and form or growing style to make it suitable for avenue planting. Flowering Gum (*Corymbia Ficifolia*) offers the best opportunity to use both full size and dwarf species to build a consistent avenue of trees within the existing conditions. In addition, this species is already common within the streets of Ocean Grove.

Other trees considered include Hoop Pine (*Araucaria Cunninghamii*) however this species will grow to a substantial size and require lopping under the powerlines or



Figure 18: Green link concept plan

- Legend
- Proposed street trees
  - Existing street trees
  - Existing footpath
  - Infill footpath required
  - - - Potential shared path

# Moving Safely

the Silver Banksia (*Banksia marginata*) but this species will take a long time to reach a size that will create an avenue.

Figure 18 shows the proposed green links and footpath upgrades required.

Figure 20 shows an artist's impression of the upgraded Presidents Avenue.

## Cycling

In terms of cycling, having considered both the BikeSafe - Principle Bike Network and Council's Cycling Strategy through the development of the UDF a proposed bike network was identified for accessing the Town Centre is shown in Figure 21.

The Terrace provides the best opportunity to improve cycling access to the Town Centre and connects to a future off-road path proposed on Tuckfield Street. This link could include either on-road lanes or an off-road bike path on one side depending on project funding and community expectations.

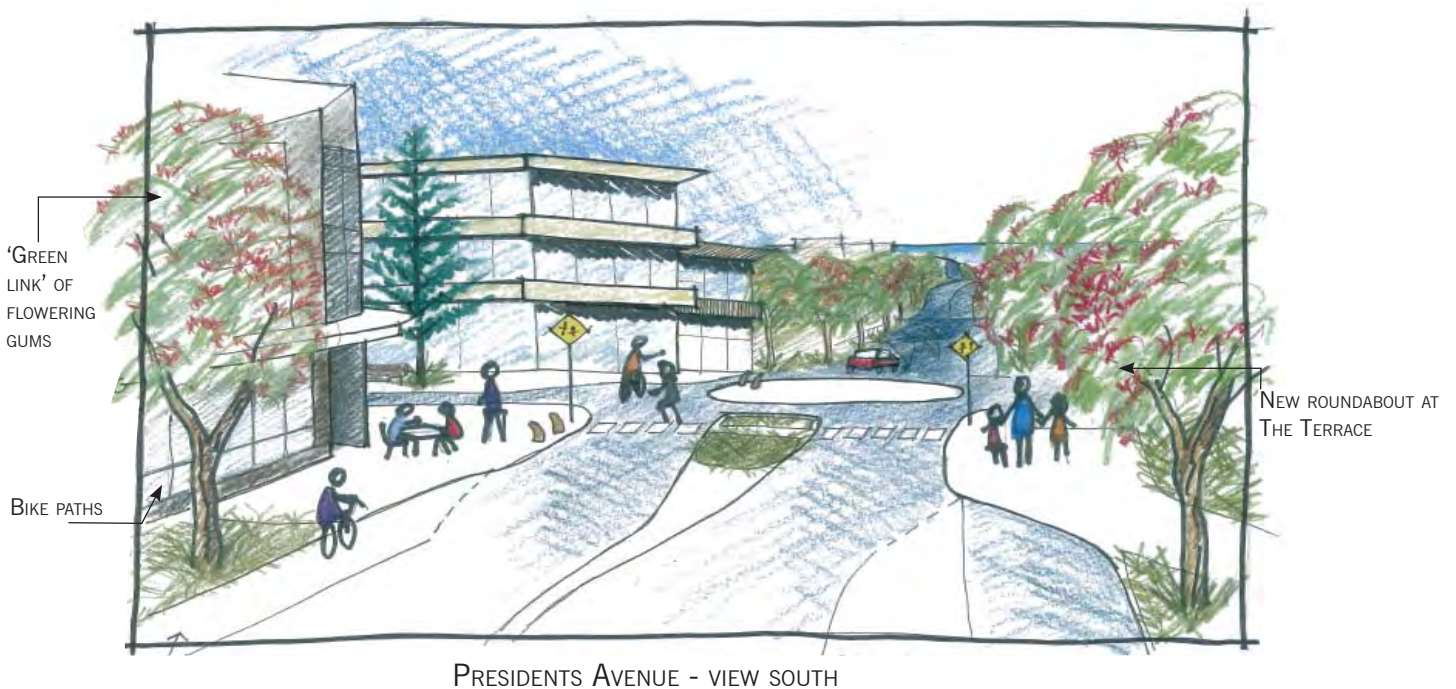
Additional bike parking is required in The Terrace to provide bike parking in the heart of the Town Centre. Council should consider the temporary or permanent removal of car spaces to provide a substantial bike storage in the The Terrace.



Figure 19: Potential 'green link' species (E.g. Flowering Gum in The Avenue)



Existing view of Presidents Avenue 'Green Link'



PRESIDENTS AVENUE - VIEW SOUTH

Figure 20: Presidents Avenue 'Green Link' - illustration by City of Greater Geelong

# Moving Safely

## Presidents Avenue / Field Street

Presidents Ave has been identified as the primary north-south cycle connection to the Town Centre, however, the existing infrastructure for cyclists is limited to a shared parking/cycle lane.

Off road bike paths and separate pedestrian paths on Presidents Avenue and Governors Place would provide the safest and most direct route to the Town Centre however this would involve the complete redesign of the kerb and nature strips and removal of vegetation on Presidents Avenue and Governors Place for construction.

The more achievable and cost effective option is a staged improvement to the existing infrastructure through:

- On-road cycle lanes on Presidents Avenue;
- Street tree planting to create strong landscape links;
- Widening and infill of footpaths on Governors Place and Presidents Avenue as demand requires;
- A public art and wayfinding signage trail from the beach.

In the longer term, the construction of a less formal north-south shared path on Field Street and The Terrace that is suitable for less confident riders and families would compliment the upgrade to Presidents Avenue.

The crossing of Orton St and right hand turn into The Terrace should be treated to provide safe access. The section of the Terrace from Field St to the town centre should be reviewed to ensure safe, easy access for all cyclists.

## Hodgson Street

There is no additional space on Hodgson Street for dedicated off-road bike paths so the green links will focus on street planting, upgrading existing footpaths and public and art and wayfinding signage from the beach.



Figure 21: Proposed cycle network

### Legend

- Town Centre
- Arterial road
- Principle bike network
- - - Secondary bike network
- Recreation bike path
- Shared area
- · · Potential shared path

# A Vibrant Town Heart

## Preferred future outcomes

- Ocean Grove has 'heart and soul' with spaces for the community to gather. It is an attractive and vibrant place with retail, business, social and cultural activities. It's big beach, ocean and surf character is evident.
- Ocean Grove has a strong connection to and respect for its many environments. Conservation, new plantings and 'green links' add to the areas special character.
- The urban structure and environment encourages people living in quality housing and working in the Town Centre.
- Ocean Grove's Design and Planning Guidelines support developments that advance the special character of Ocean Grove.



# A Vibrant Town Heart

## Opportunities and Constraints

### Precincts

There are three distinctive precincts within the Town Centre that should be considered separately to ensure their distinctive character is enhanced. The precincts are shown in the Precinct Plan at Figure 22.

These are:

- **Precinct 1 - The Terrace - Retail, tourism & residential**
- **Precinct 2 - The Avenue - Office & residential**
- **Precinct 3 - Hodgson Street - Mixed Use**

Within each precinct key projects and design guidelines are identified and described later in the chapter. The Ocean Grove Town Centre will be strengthened, expanded and promoted as the commercial and civic heart of Ocean Grove. Building on the Ocean Grove Structure Plan, a wide range of retail and tourist related activities, as well as complementary apartments and tourist accommodation will be encouraged to give life and vitality to the Centre during and outside business hours.

Residential growth and the development of new retail centres will provide more competition so there must be an improvement to the quality of the experience of the Centre to retain and attract locals and visitors.



Figure 22: Town Centre - Precinct Plan

# A Vibrant Town Heart

## Economic Opportunities

Worldwide evidence<sup>1</sup> suggests that investments to improve walking and cycling environments have an average economic benefit of 13:1. Attractive streetscapes increase pedestrian activity, have higher visitation levels and encourage people to spend more time to take in shop fronts, and this translates into increased retail spend.

Further, it has been found that locally-owned businesses are particularly successful in walkable communities, and that \$1 spent at an independent business generates about three times as much benefit to the local economy as \$1 at a chain retailer.

Studies consistently show that retailers hugely overestimate the importance of the car, and underestimate the number of their customers who rely on walking and cycling.

International research has also showed a link between high quality streetscapes and increased retail and private property values, which attracts new tenants and businesses.

The improved amenity of the public realm encourages not just healthy activity such as walking and cycling, but also broader social benefits – people sitting together, meeting neighbours, and feeling connected.

Mixed-use development of existing sites within the Town Centre will strengthen the economic diversity to the centre. To stimulate development and regenerate older building stock a component of new development will be residential to underpin the economic feasibility of the development.

Based solely on projected population growth and the existing approved retail floorspace in the retail catchment there is limited demand for additional retail floorspace. Notwithstanding this, if the market sought to provide additional retail floorspace in the catchment it should be located in the Town Centre to consolidate it as the primary retail centre for Ocean Grove.

The demand for future office space will be driven by population growth. Estimated demands between 2013 and 2031 is an additional 800 sqm based (approximately) on the per capita floor space allocation that currently exists<sup>2</sup>.

The existing commercial land in the Town Centre can accommodate up to an additional 9,000 sqm of floorspace for retail, office, community and medical uses. The total existing floorspace of all land uses is approximately 21,500 sqm. Therefore, the existing supply of commercial land is sufficient to meet floorspace demands to 2031 and beyond. The rezoning of any additional commercial land should only be considered as part of a consolidation and development of existing sites within the Town Centre such as the reuse of surplus government land.

The promotion of residential development within the Town Centre will increase both the permanent and visitor population, supporting existing business and reducing

vehicle trips to the Town Centre. Between 50 and 100 additional dwellings could be accommodated within the Town Centre within the upper floors of new and existing developments.

## Community Facilities

The Council Customer Service Centre / Library and Bellarine Community Health Service are co-located in a busy centre on the corner of The Avenue and Presidents Avenue. The maternal and child health centre will shortly move to the new Integrated Children's Centre on Shell Road creating opportunity for additional space in the existing centre.

Through the EbD and consultation process for the development of the UDF it was evident that there was strong community support for additional community facilities in Ocean Grove, particularly a civic hall or meeting space. As the community of Ocean Grove continues to grow additional community floorspace may be required to accommodate the community's needs however there are no plans to change the existing service arrangements at this time.

The existing Ocean Grove Neighbourhood House is located at 91 The Parade. The Neighbourhood House will be relocated to the redeveloped child care centre on Shell Road. This site is owned by Council and is surplus to Council's requirements. This site will be sold for commercial development and could form part of a consolidated redevelopment of the land on Hodgson Street, north of The Parade.

The Ocean Grove Senior Citizens Centre and Community Garden occupy a double block between The Terrace and The Parade. The site is ideally located for community uses. The ongoing shared use by a number of community groups will compliment the range of services in the Town Centre.



Figure 23: Ocean Grove Council Customer Service Centre

1 (Tolley, Dr Rodney "Good for Busine\$\$" Heart Foundation 2011)

2(Urbacity, Ocean Grove Centre Study, April 2013)

# A Vibrant Town Heart

## Significant Views

The Town Centre is located approximately 33 metres above sea level. The terrain rises steeply on approach to The Terrace offering sought after views to the ocean and the river. The urban design framework seeks to protect key views from the public realm and maximise opportunities to capture views. Building heights on key prominent corners have been limited to achieve these objectives.

Figure 24 shows the significant views to be protected and enhanced.

## Landscaping

Landscaping can be used to create distinctive precincts or themes within a space and highlight entrances. There was strong community support for further street tree planting and an overall greening of the Town Centre.

Ocean Grove has stunning landscape setting where the public and private realm and blurred with low and dense vegetation. The recurrence of distinctive coastal vegetation such as Moonah (*Melaleuca lanceolata*) and the robust

Coastal banksia (*Banksia integrifolia*) tie the Town Centre to its coastal setting.

The distinctive Norfolk Pines (*Araucaria heterophylla*) clearly define the shopping precinct and the indigenous Yellow Gum (*Eucalyptus leucoxylon ssp bellarinensis*) creates a natural setting on The Avenue.

New landscaping should:

- Distinguish character areas with the Town Centre by building on the existing landscape elements.
- Provide protection from the sun and wind and soften the dominance of hard surfaces and car parks.
- Reinforce a sense of human scale and enclosure to the Town Centre.
- Strengthen the connections to the beach and the park.

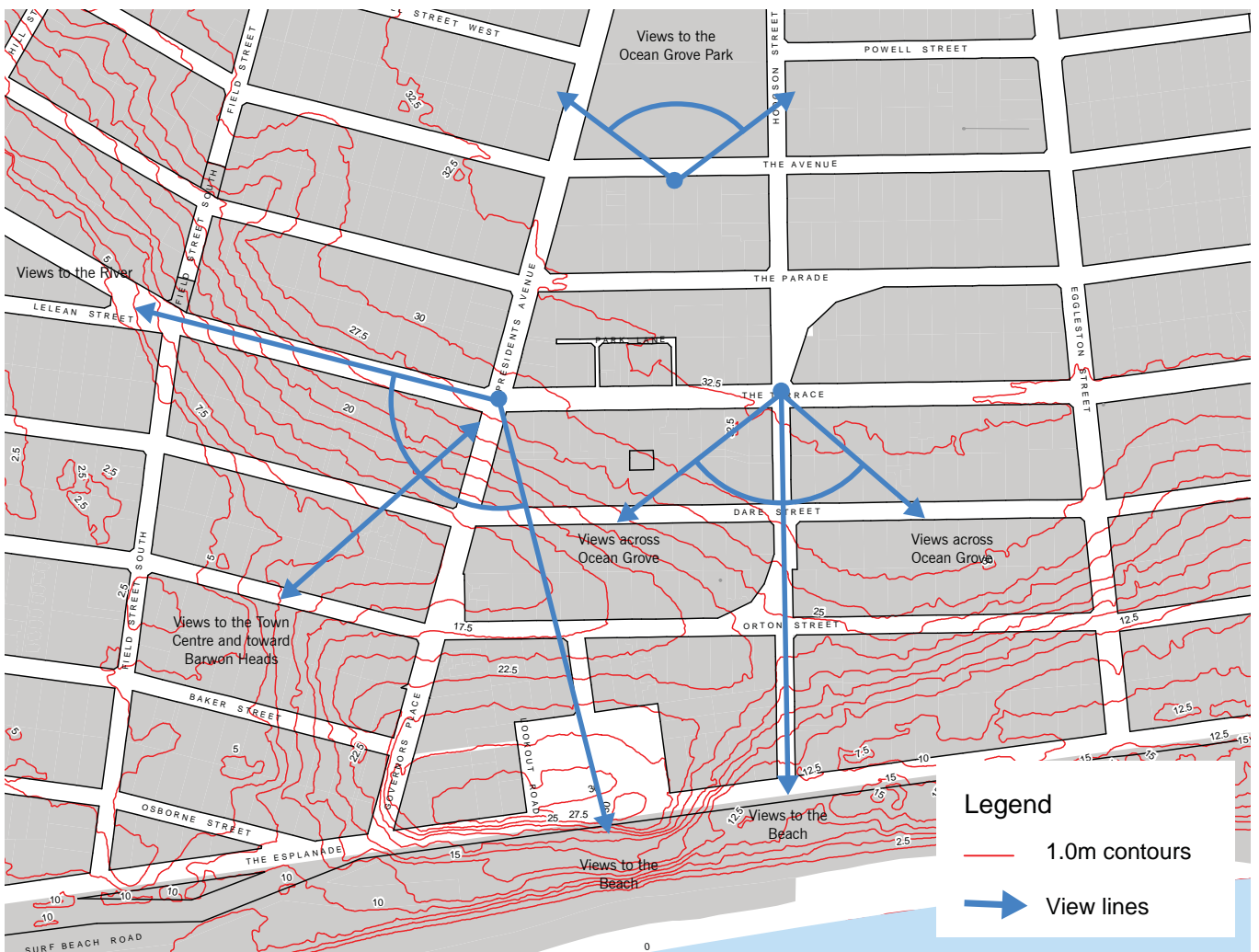


Figure 24: Significant views and contours map

# A Vibrant Town Heart

## Shadows

The impact of shadows on the public realm was a key concern identified by the community as part of the community consultation. Figure 25 shows the extensive analysis that has been undertaken to assess the impact of new development on the shadows in the streets and minimise future impacts.

To minimise overshadowing development on the northern side of The Terrace is limited to three storeys and the third storey must be recessed to minimise the bulk of the buildings at higher levels.

The recessing of third and fourth storeys in all other areas will limit the impact of overshadowing. The diagram below shows the shadow cast by new and existing buildings at the September equinox and winter solstice.

Winter sun is particularly important in the street and solar access to The Terrace. The proposed design controls for development on the north side of the The Terrace will ensure that sun light reaches the opposing footpath from 11am on the shortest day of the year.



Shadows - 22nd June - 11am



Shadows - 22nd September - 11am



Shadows - 22nd June - 3pm



Shadows - 22nd September - 3pm

### Legend

- Existing shadow
- Overlap of existing and new shadow
- New shadow

Figure 25: Long term shadow analysis based on full build out of likely development sites

# A Vibrant Town Heart

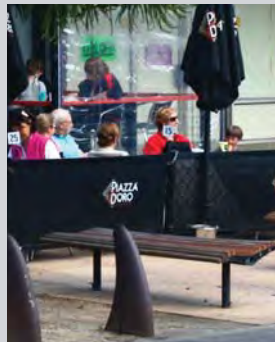
## Enhancing Character

The Town Centre is characterised by a relaxed atmosphere and a strong connection with the beach and surf culture. The local community values the 'High Street' or village character of the low scale, open air repeating shop fronts

that make for a vibrant street environment. The existing Norfolk Pines and fin bollards in The Terrace are uniquely Ocean Grove.

There is no single building age, type or character that defines what a future Ocean Grove character should be. What is important is that new buildings prioritise

### Streetscape elements



Fin bollards - iconic street furniture & neutral paving



Dense native landscape plantings



Vibrant street life

### Built form character elements



Variety of natural materials



Non dominant scale



Modest, simple building form



Glimpses to water



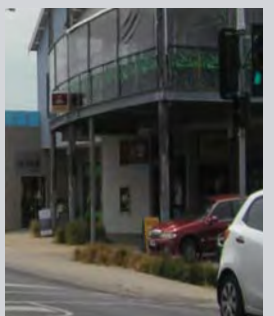
Active frontage & alfresco dining



Dominant vegetation



Avoid blank walls in public areas



Avoid - cars dominating frontage

# A Vibrant Town Heart - Design Guidelines - General

pedestrians, create interesting and vibrant streets and sit within the unique landscape. Avoiding new development that priorities the car and does not contribute to the street environment should be the imperative.

## Design guidelines - General

The following design guidelines for all buildings are provided to encourage an innovative and contemporary approach to building design in the Ocean Grove Town Centre to deliver the preferred future outcomes of the Town Centre.

In addition, specific guidelines have been developed for the distinctive precincts within the Town Centre that are

provided later.

## Building Heights and Setbacks

The low rise scale of the Town Centre is a characteristic valued by the community and an important element of the existing village character. The heights of existing buildings allows the sun to reach the footpaths and provides glimpses to water views. To maintain the existing character and avoid overshadowing of the public realm building heights should not exceed the preferred maximum height limits above natural ground level and setback standards set out in Figure 26.



### Legend

#### Heights

- 13.5m - up to four storeys
- 10.5m - up to three storeys
- 9.0m - up to three storeys
- 10.5m - up to three storeys (surrounding residential increased housing diversity area)

#### Setback standards

- Standard 1
- Standard 2
- Standard 3
- Standard 4
- Standard 5
- Standard 6

Figure 26: Town Centre - Preferred maximum building heights

# A Vibrant Town Heart - Design Guidelines - General

In developing the building height guidelines, key considerations have been:

- Recessed 4th storey on The Terrace out of sight so it is not visible from the other side of the street;
- Reinforcing The Terrace as the prominent shopping strip; and
- Minimising visual impact and bulk from key viewing locations to the south by recessing buildings from southern boundary on The Terrace and limiting building height and siting on corner sites.

Higher elements above the preferred maximum heights may be considered where a specific urban design benefit can be demonstrated. The building must respect the preferred streetscape character, minimise shadowing impacts on adjoining properties and public areas and advance high quality coastal design and environmentally sensitive design principles.

Six setback standards have been developed that are applied within each precinct to guide development.

These building design guidelines apply to all precincts.

## Building Design

- Buildings should demonstrate a high quality urban design, incorporating contemporary architectural elements that complement the coastal character of Ocean Grove.
- Active uses should be provided on frontages identified in the overall framework plan on page 11. On other frontages the built form should address the street and make a contribution to the activation of the streetscape.
- Blank walls are discouraged on façades fronting public areas. Where blank walls cannot be avoided, windows or openings should be provided to promote interaction with the street. If windows or openings are not appropriate other design treatments may be considered to break up the surface and provide visual interest and amenity such as mural painting or vertical gardens.
- The colour palette of new development should reflect the vegetated landscape character of Ocean Grove.
- A variety of buildings materials should be used to provide visual interest and break up the bulk of upper levels.
- Lighter looking building materials such as timber are encouraged. The use of exposed brick and masonry cladding should be avoided.
- Ensure that built-form minimises impacts to solar access to balconies of adjoining buildings and sites.

- Utilise construction materials and products that incorporate low-embodied energy, are robust, and weather resilient, and are low maintenance to maximise their life-cycle.
- Maximise passive opportunities for energy efficiency in building siting and design.
- Balconies and upper floor setbacks should be provided to reduce the visual bulk and dominance of development. Balconies should be 30% transparent.



Figure 27 : Coastal banksia planting in The Parade car park

## Access and Parking

- Parking associated with residential development should be integrated with the development.
- Avoid use of pedestrian fences. Use landscaping to discourage mid-block crossing.
- Wayfinding signage should be provided to direct visitors to car parking.
- Existing public car parks should be maintained or offset within close proximity to the Town Centre.
- Limit vehicular crossing points to one cross over per property and encourage the sharing of access points where possible.
- New carparks or carpark upgrades should include water sensitive urban design treatments for stormwater.

## Landscaping

- Landscaping that enhances the coastal character of Ocean Grove should be provided as part of new developments. Dense understorey planting and moderate size canopy trees are encouraged.
- Landscaping should include hardy plant varieties that are suitable for a coastal environment and include a combination of the trees, shrubs and ground covers selected from the City of Greater Geelong, Indigenous plants of Geelong Region (1999).
- On-street planters that define individual properties and on street dining areas are encouraged to contribute to the greening of the streets provided they do not impede the movement of pedestrians.
- Extensive use of trees is promoted throughout car parks to provide shelter and break up the visual impact of hard surfaces. Dense under-storey planting of shrubs and grasses is recommended to strengthen the existing coastal themes. Public car parking areas should provide trees that provide a canopy cover of at least 25% of car park areas at maturity.
- Landscaping such as planter boxes should be provided on balconies and roof tops.
- Streetscape and drainage upgrades should incorporate water sensitive water design treatments in the design to improve water quality from the Town Centre.

## Advertising Signage

Advertising signage is a dominant characteristic within the Town Centre. Whilst some signage is required to identify businesses it is critical that new signage does not dominate the streetscape and detract from the coastal setting.

It is particularly important that signage does not detract from the Ocean Grove Park or interrupt views to the Ocean.

Signage should be located below verandahs or incorporated within the building facade in an innovative way. Signage on the back or side walls of buildings is discouraged, particularly at upper levels.

The design and colours of signage should respect the coastal vegetated setting. Animated and electronic signage will distract from the coastal setting and should be avoided.

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The rest of the chapter describes the Design Objectives, Projects and Design Guidelines for each of the precincts.

# Precinct 1 - The Terrace

## Design Objectives

The Terrace and Park Lane will continue to provide the primary retail heart of the Town Centre. On-street dining and active frontages will continue to provide vitality and vibrancy to the street.

Pedestrians have priority in the streets and car parking areas and slow traffic through The Terrace enhances the activity on the street.

As older shops are replaced, new development should continue the open air, repeating narrow shop fronts. The Norfolk Pines dominate the streetscape and further plantings will enhance the iconic streetscape theme.

To protect views to The Terrace from the beach and Barwon Heads Road and ensure sunlight reaches the street upper building levels should be recessed and minimise visibility from the opposite side of the street.

Development in The Terrace will take advantage of the magnificent big sky and water views.

New residences above the shops in The Terrace and Park Lane will be encouraged with balconies to promote surveillance of the street and rear lane and outdoor living.

Architectural styles should be contemporary and innovative and build on the colours and materials of the surrounding landscape and simple forms of the traditional shop fronts and beach shacks.

The construction of the rear lane-way on the south side of The Terrace will facilitate residential development of both the commercial properties in The Terrace and the residential properties in Dare Street.

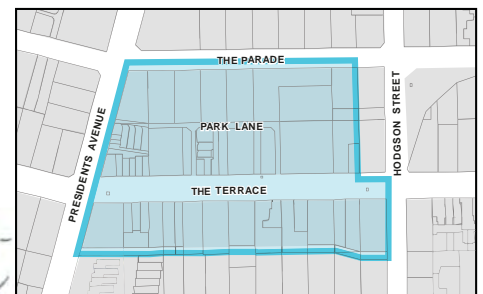


Figure 28: Artist's impression of The Terrace - illustration by Peter Edgeley

# A Vibrant Town Heart - Precinct 1 - The Terrace

Within The Terrace precinct there are two streetscape projects; the Town Square and rear lane-way connection. Specific design guidelines for the precinct have also been developed.

## Projects

### Town Square

The creation of a town square will create a pause point that is a sunny resting place in The Terrace. The space could be used for on-street dining, visitor information or community displays, seating and public art.

By using part of the road reserve and setting back a future building a intimate and achievable public space can be created.

In addition, there is space in the street for the planting of two additional Norfolk Pines that will further enhance the greening of the street.

Additional short term car parking can be achieved by converting the existing parking on the north side of the street to 60 degree parking.

Additional bike parking should be provided as part of the streetscape works, including the temporary and permanent use of car parking spaces for additional bike parking in convenient locations for cyclists in The Terrace.

### Rear Laneway Connection

The rear laneway south of The Terrace provides an opportunity to:

- Remove unnecessary loading and unloading from The Terrace;
- Remove redundant driveways in The Terrace to improve pedestrian safety and increase car parking capacity; and
- Create opportunity for the redevelopment of properties in Dare Street and The Terrace with rear loaded garages.

Figure 30 provides a concept plan for both projects.



Existing Town Square site

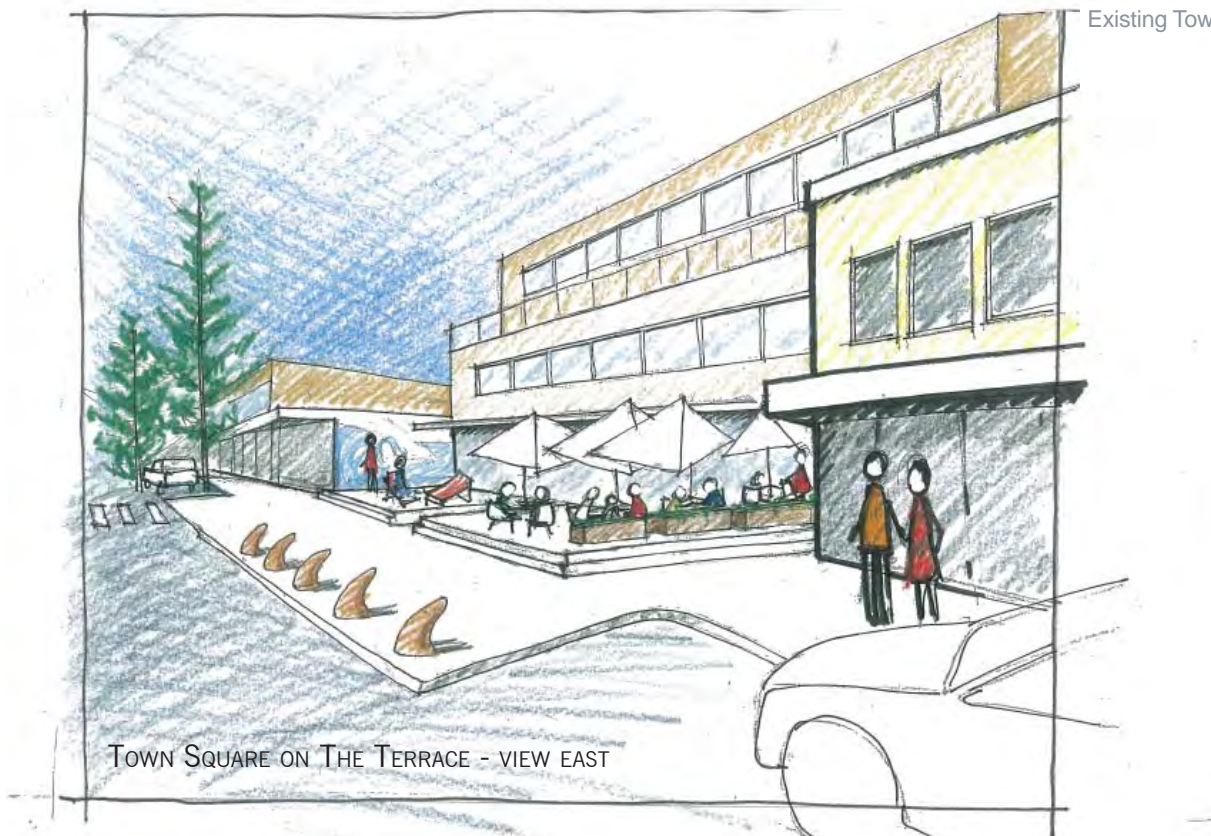


Figure 29: Artist's Impression of the Town Square - illustration by City of Greater Geelong

# A Vibrant Town Heart - Precinct 1 - The Terrace

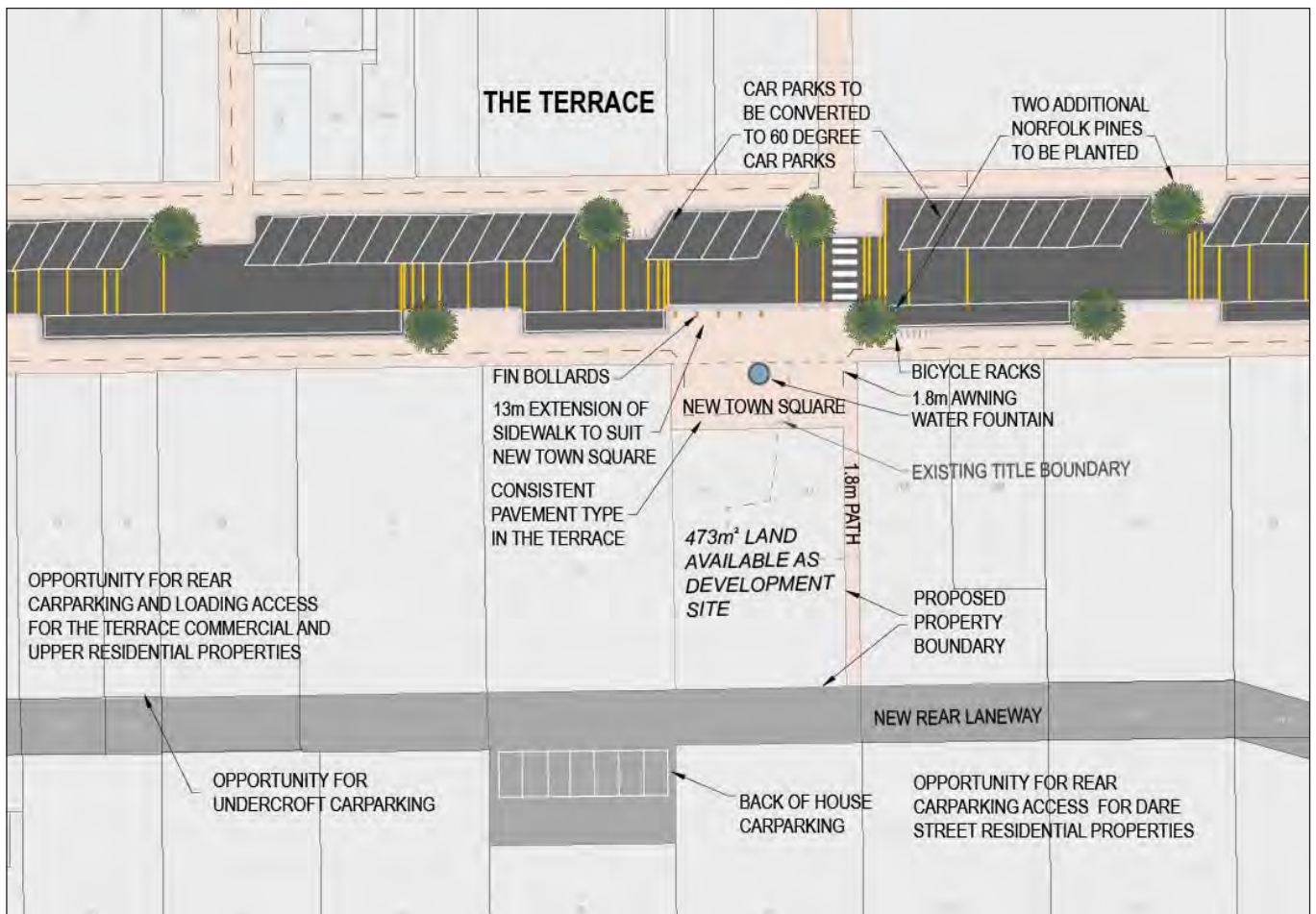


Figure 30: The Town Square and rear lane way concept plan

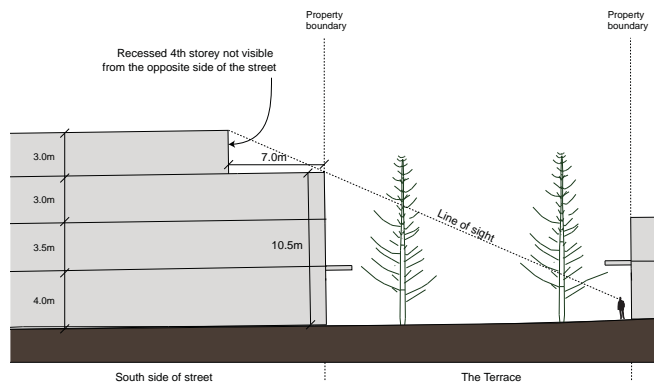
# Town Heart - Precinct 1 - The Terrace

## Design guidelines

### Setback Standards

#### Standard 1 - South side of The Terrace

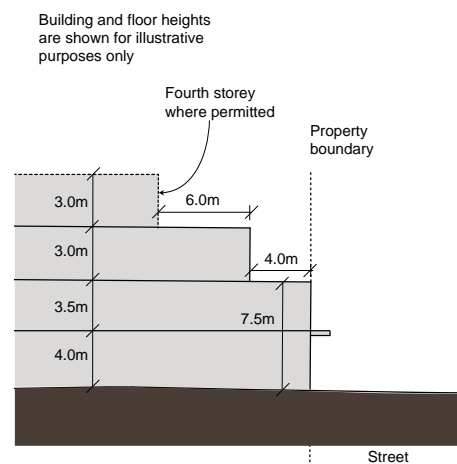
- Buildings should be constructed to the property boundary, unless civic space is integrated with the footpath.
- A corner splay to 5m x 5m must be provided on the south east corner of The Terrace and Presidents Avenue to provide views to the water.
- At the property boundary fronting The Terrace a building can be constructed to a maximum height of 10.5 metres.
- Buildings should be setback 6.5m above a height of 10.5m to obscure the view of a fourth storey from ground level on the opposite side of the street.
- Buildings at 74 & 74A The Terrace must be setback 7 metres to allow for creation of the Town Square.



Setback standard 1

#### Standard 2 - North side of The Terrace

- Buildings should be constructed to the property boundary, unless civic space is integrated with the footpath.
- At the property boundary fronting The Terrace a building can be constructed to a maximum height of 7.5 metres.
- Buildings should be setback 4m above a height of 7.5m to reduce the shadowing on the street.

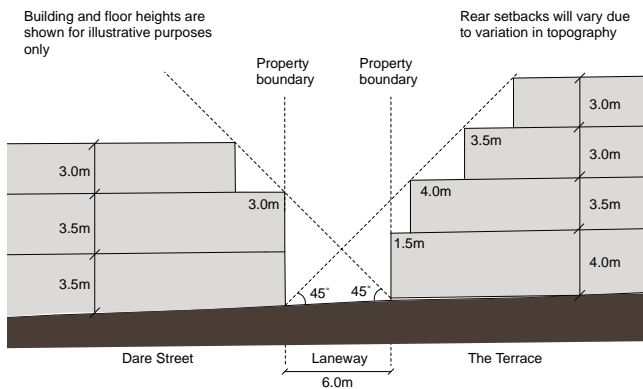


Setback standard 2

# A Vibrant Town Heart - Precinct 1 - The Terrace

## Standard 3 - Rear lane-way

- A maximum height at street edge of 6m (equivalent to the street width).
- Above 6m the building height must be in accordance with a 45° angle from the opposing property boundary as shown.



Setback standard 3

## Building Design

### The Terrace & Park Lane

- Active pedestrian orientated frontages must be provided on a facade fronting The Terrace, Park Lane or a connecting lane-way.
- Buildings should include simple cantilevered awnings to reflect the prevailing character of awnings in The Terrace.
- At ground floor level shop fronts should provide interaction with the street with a high proportion of glazing to promote visibility to stores.
- Retractable doors are encouraged to provide interaction with outdoor and indoor dining areas.
- Where larger buildings are proposed the traditional image of multiple repeating shop fronts should be maintained.
- 62 & 63A The Terrace are highly visible from the beach and surrounding public areas. A high quality signature entry building is required that responds to the prominence of the site within the surrounding landscape.

### In the rear lane way:

- New development should include lighting of the lane way to promote safety.
- Balconies and roof top areas above first floor level are encouraged to promote surveillance of the lane way.
- Balconies at the first floor level must not extend beyond the front wall of the ground floor.
- New development should include a variety of materials at ground floor level to break up the building mass. Small landscaped areas in the lane way are encouraged.

## Access and parking

- No vehicle access from The Terrace will be supported.
- Existing driveways in The Terrace should be removed.
- Loading and unloading should occur in the rear lane way or Park Lane.

# Precinct 2 - The Avenue

## Design Objectives

The Avenue and The Parade offer a transition from the primary retail heart to the open spaces of the Ocean Grove Park and the surrounding residential areas. This will be further accomplished by the continuation of the north-south pedestrian link between the proposed Town Square, Cenotaph and the Park.

A mix of retail, commercial and residential uses will coexist. Flexible design will enable alternative uses and adaptations of new buildings in the long term as demand for retail and commercial floorspace changes.

The design of built form on The Avenue will be responsive to the Park, maximising the views and incorporating significant vegetation, with set backs from the street for tree-planting. A good example is the current café overlooking the Park. Upper

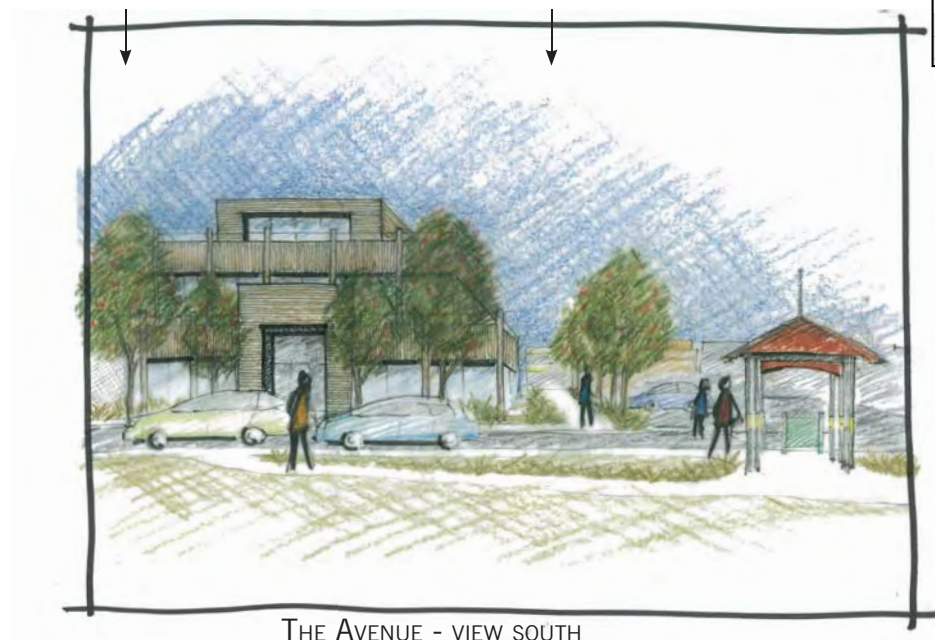
levels will be recessed at the front and sides to ensure the streetscapes are not dominated by solid built forms, and the sense of open space is retained.

New development on The Avenue is envisaged as boutique office space, with residential living above, taking advantage of the superb views overlooking the Park.

The street upgrade of The Avenue will result in an "avenue", lined with major trees at regular intervals on both sides and with footpaths. Further street tree plantings in The Avenue should connect with the native gums in the Park such as Lightwood (*Acacia implexa*) or Black Sheoke (*Allocasuarina littoralis*) or continue the planting of Flowering gums (*Corymbia Ficifolia*).

NEW DEVELOPMENT TO  
OVERLOOK PARK & CAR PARK

IMPROVED PEDESTRIAN CONNECTION  
THROUGH TO THE TERRACE



THE AVENUE - VIEW SOUTH



isting vie

Figure 31: Artist's impression of The Avenue looking south - illustration by City of Greater Geelong

# A Vibrant Town Heart - Precinct 2 - The Avenue

Within The Avenue precinct there are two streetscape projects; streetscape works in The Avenue and improvements to the existing pedestrian link. Specific design guidelines for the precinct have also been developed.

## Projects

### The Avenue Upgrade

The upgrade will improve the overall amenity of The Avenue and act as a catalyst for new investment and redevelopment. The opportunities are:

- Significant tree-planting in nature strips to create an 'Avenue';
- Construction of footpaths on both sides of the street;
- Indented parking in The Avenue and Presidents Avenue adjacent to the Ocean Grove Park for use as all day parking; and
- Shared path or on road cycle lanes in Presidents Avenue to ensure cyclist safety and upgrade existing poor quality footpath.

Figure 33 provides a concept plan for the upgrades to The Avenue.

### Pedestrian Link

An improved pedestrian link between Ocean Grove Park and the Town Centre will provide both a physical and visual connection, encouraging increased access between the two key points.

The opportunities are:

- An upgrade to the existing pedestrian connection through existing car park to include landscaping.
- New development to address the pedestrian link to promote visual interest and amenity.
- Raised pedestrian crossings in The Parade and The Avenue.

The Concept Masterplan from the Enquiry by Design workshop showed a link further to the east, however this link would require the acquisition of private property and it is considered that an improvement to the existing connection will achieve the desired outcomes and minimise project costs.



Figure 32: New development to enhance character of the Park similar to the existing Cafe



Figure 33: Concept plan for The Avenue Streetscape upgrade

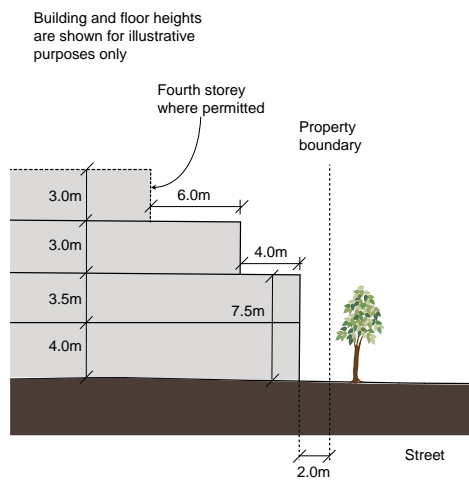
# A Vibrant Town Heart - Precinct 2 - The Avenue

## Design guidelines

### Setback Standards

#### Standard 4 - The Parade & Hodgson Street

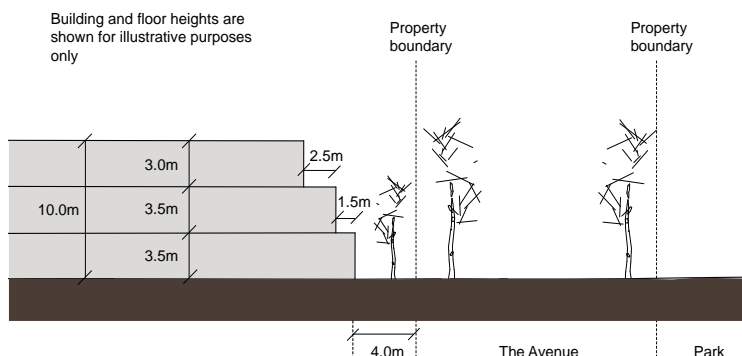
- Buildings should be setback from the property boundary 2m.
- At the property boundary fronting a building can be constructed to a maximum height of 7.5m.
- Buildings should be setback 4m above a height of 7.5m to reduce the visual dominance of the upper level.



Setback standard 4

#### Standard 5 - The Avenue

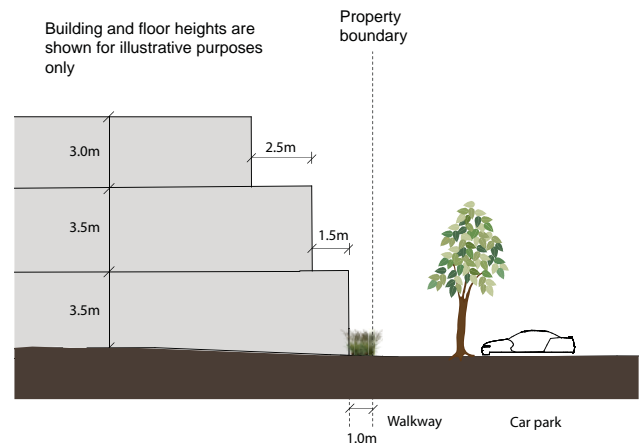
- Buildings should be setback from the property boundary 4m.
- Buildings should be setback a minimum of 1.5m above a height of 4.0m and a further 2.5m above 7.0m to provide articulation on the facade.
- Space should be provided between buildings for the provision of landscaping.



Setback standard 5

#### Standard 6 - Pedestrian linkage

- Buildings should be setback from the property boundary 1.0m.
- Buildings should be setback a minimum of 1.5m above a height of 4.0m and a further 2.5m above 7.0m to provide articulation on the facade.



Setback standard 6

# A Vibrant Town Heart - Precinct 2 - The Avenue

## Building Design

### The Avenue

- Built form should address the street and the Park and include articulated design and landscaping to provide visual interest.
- Landscaping in front and side setbacks should provide a strong connection with the Park.
- Buildings should include balconies and roof top terraces to provide passive surveillance of the Park.
- Ground floor residential uses are supported however the built form should provide for the future adaptation of the buildings for office or retail uses.
- Buildings should include a variety of roof forms and materials to provide a transition from the commercial areas to the surrounding residential areas.
- Buildings should use a combination of glazing and solid materials at ground floor.

### Pedestrian Linkage

- Built form should address the walkway and provide visual interest and articulation.
- Development should provide a landscape buffer to the car park that is integrated with the footpath and public realm.

### Hodgson Street and The Parade

- All built form should address the street and include articulated design and landscaping to provide visual interest.
- Front setbacks on Hodgson Street should be integrated with the footpath and public realm.
- Ground floor residential uses are supported however the built form should provide for the future adaptation of the buildings for office or retail uses.

## Access and Parking

- Any new parking facilities fronting the park should provide a dense landscaping buffer that enhances the public realm and connection with the Park.
- Car parking should be consolidated and shared by multiple properties where possible. Car parking should be located to the rear of buildings and have minimal impact on the streetscape.
- Maintain access to the existing car park from both The Avenue and The Parade to disperse traffic.
- Development at 89 & 91 The Parade should be from Hodgson Street. If necessary, access to The Parade, east of Hodgson Street should be restricted to left turn in and left turn out.

# Precinct 3 - Hodgson Street

## Design Objectives

The purpose of the Hodgson Street upgrade is to reinvigorate the Town Centre, enhance the public realm – particularly for pedestrians – and stimulate investment.

The new configuration of Hodgson Street creates a principal boulevard, with significant tree planting and landscaping along each side and on the median strip. The wide footpath that continues Town Centre theme of coloured paving and fins, will have spectacular views to the ocean.

Pedestrian convenience and safety will be prioritised, with the squaring up of the intersection at The Parade/Hodgson Street and slower traffic speeds on Hodgson Street. Bus stops will have better visibility and access to promote public transport options.

The changed road alignment offers the opportunity for a major development on the east side, which could include the missing entertainment and hospitality features of Ocean Grove Town Centre, such as a cinema and tourist accommodation.

The location, at the top of the ridge, enables a high quality three to four story development with extensive ocean views, clearly differentiated from the lower scale, retail-oriented area of The Terrace and the office/residential area of The Avenue.

At street level Hodgson Street will have activated frontages continuing the small, repeating shop fronts of The Terrace with awnings and al-fresco seating with ocean views. Setbacks, balconies and articulated design for the upper levels will ensure sunlight to the street and retain the local character.

New planting in Hodgson Street will provide for shaded meeting areas the street to create a unique new precinct. Tree planting could continue the 'green link' planting theme, Coastal banksia (*Banksia integrifolia*) or include more traditional shopping strip street trees such as White Cedar (*Melia azedarach*) depending on community preferences.

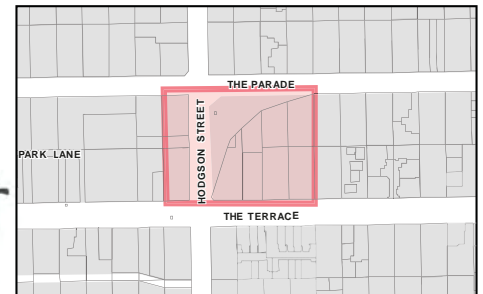


Figure 34: Artist's impression of Hodgson Street - illustration by City of Greater Geelong

# A Vibrant Town Heart - Precinct 3 - Hodgson Street

Within the Hodgson Street precinct the primary project is the substantial redevelopment of Hodgson Street. Specific design guidelines for the precinct have been developed and the testing of two options for how the UDF could be applied to the development site is provided at the conclusion of the chapter.

## Projects

### Hodgson Street Upgrade

The upgrade will improve the overall amenity of Hodgson Street and encourage further investment and redevelopment.

The upgrade will include:

- Improved pedestrian safety through the squaring up of the intersection;
- Creation of 'green link' on Hodgson Street with street trees and landscaped central median;
- Relocation of bus stops;
- Suitable space for service vehicles included where appropriate; and
- Activation of commercial land in Hodgson Street to create active retail frontages.

Figure 35 shows the streetscape upgrade to Hodgson Street.



Figure 35: Hodgson Street streetscape upgrade concept plan showing the street layout using signals

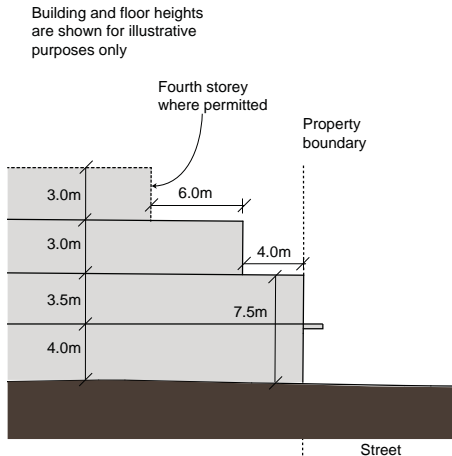
# A Vibrant Town Heart - Precinct 3 - Hodgson Street

## Design guidelines

### Setback Standards

#### Standard 2 - Hodgson Street

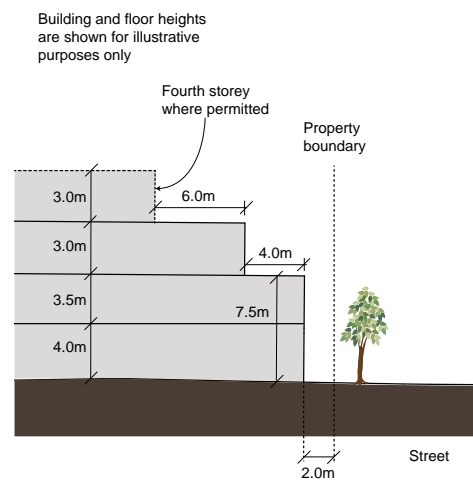
- Buildings must be constructed to the property boundary, unless civic space is integrated with the footpath.
- A 3.0m wide unobstructed footpath should be achieved at all times.
- At the property boundary fronting The Terrace a building can be constructed to a maximum height of 7.5m.
- Buildings should be setback 4m above a height of 7.5m and a further 6.0m at a height above 10.5m.
- A corner splay to 5m x 5m must be provided on the north west corner of The Terrace and Hodgson Street to provide views to the water and provide for future road widening.
- A corner splay to 7m x 4m must be provided on the north east corner of The Terrace and Hodgson Street to provide views to the water and provide for future road widening.



Setback standard 2

#### Standard 4 - The Parade and The Terrace

- Buildings should be setback from the property boundary 2m.
- At the property boundary fronting a building can be constructed to a maximum height of 7.5 metres.
- Buildings should be setback 4m above a height of 7.5m to reduce the visual dominance of the upper level.



Setback standard 4

# A Vibrant Town Heart - Precinct 3 - Hodgson Street

## Building Design

- Active pedestrian oriented frontage must be provided on a facade fronting Hodgson Street or a connecting lane-way.
- Buildings fronting Hodgson Street should include cantilevered awnings.
- At ground floor level shop fronts should provide interaction with the street with a high proportion of glazing to promote visibility to stores.
- The traditional image of multiple repeating shop fronts should be provided on Hodgson Street.
- Retractable doors are encouraged to provide interaction with outdoor and indoor dining areas.
- Where possible the development layout should direct pedestrian movements to the existing signalised intersection at The Terrace and Hodgson Street.
- Front setbacks on The Parade and The Terrace should be integrated with the footpath and public realm.
- A comprehensive development of the precinct should include an east-west pedestrian connection to Hodgson Street.
- Development fronting a pedestrian linkage within the precinct should be designed to minimise overshadowing of the public realm and include openings and visual interest for pedestrians.
- Subdivision and development must not jeopardise the future conversion of The Terrace to the arterial road route.
- Development should contribute to the enhancement of the public realm and streetscape upgrades, such as the inclusion of public art at prominent sites.
- Development should establish a high quality design outcome that responds to the site area as the eastern entry to the Town Centre and takes into consideration the surrounding landscape setting and ocean views.
- On upper floors vary the setback of the front wall to create articulation and break up the bulk of the building.

## Access and Parking

- Vehicular access must be provided from The Parade or The Terrace, or both. No vehicle access from Hodgson Street.
- Large service vehicles should enter and exit the site in a forward direction. An access aisle through the site is encouraged as an efficient use of space for loading and unloading.
- Maximise distance between access points and arterial road intersections.
- Car parking should be consolidated and shared by all properties. Car parking should be located to the rear of buildings or underground. Some parking may be visible on The Terrace or The Parade to provide a visual cue to a larger rear or underground car park.
- A large redevelopment should include a public car park that provides convenient, safe and attractive pedestrian connections to Hodgson Street and crossing locations at The Parade and The Terrace.
- Angle parking on The Terrace frontage is supported until such time as the arterial road is shifted to The Terrace.
- Access to development west of Hodgson Street should be through Council's car park or The Parade.

# A Vibrant Town Heart - Precinct 3 - Hodgson Street

## Development site

The squaring up of the intersection will realise new commercial land in the heart of the Town Centre. This land, in-conjunction with the Council owned assets east of Hodgson Street creates a number of development scenarios.

In a climate where funding for infrastructure is highly competitive innovative approaches are required to fund projects. The creation of a development site and a public sale process would be the catalyst for the funding of the intersection and streetscape works.

The straightening of the Hodgson Street frontage and the number of under developed properties in the precinct could create the stimulus for a comprehensive development of the land east of Hodgson Street.

The Hodgson Street East Precinct presents the best opportunity to consolidate a site that is most likely to accommodate a second supermarket for the centre.

These concept drawings illustrate two ways in which the development site and broader precinct could be developed in accordance with UDF principles. Figure 37 shows the development of the council park and development site and Figure 38 shows the overall development of the precinct.

These concept plans are examples only and are not intended to preclude consideration of the many other layouts that could be achieved in accordance with the principles of the UDF.



Existing view



EXISTING POST OFFICE SITE REDEVELOPED HODGSON STREET - VIEW NORTH

Figure 36: Artist's impression of Hodgson Street - illustration by City of Greater Geelong



Figure 37: Example of individual development site



Figure 38: Example of comprehensive development option

# Managing Parking

## Preferred future outcomes

- *Ocean Grove has a well-structured car-parking regime. There are enough parking spaces, access and circulation is easy and well-managed.*



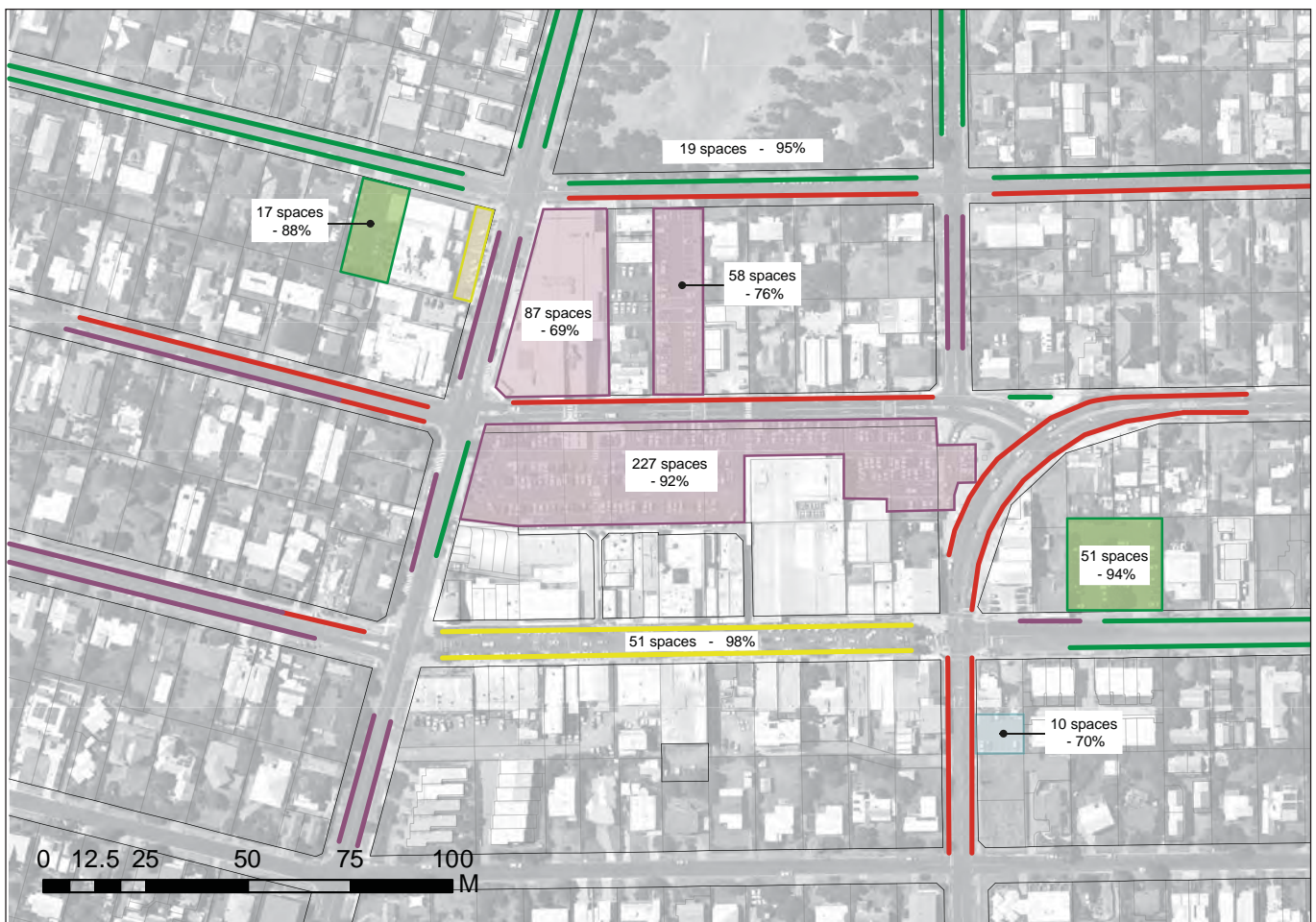
# Managing Parking

## Opportunities and Constraints

### Supply and Demand

A total of 456 public off street car parks and 235 spaces are provided on-street. There is high demand for parking within the Town Centre. On the 2nd January, one of the busiest days of the year, a peak occupancy of 91% was recorded. On December 6th a peak occupancy of 82% was recorded. Centrally located parking within The Terrace and main off-street car park experience occupancy rates above 90% at peak times. Available parking spaces and occupancy survey results are shown in Figure 39. The peak parking demand is equivalent to 3.4 spaces / 100 square metres of floor area.

It is not good practice to base planning decisions on an absolute peak that may only occur a few days per year. For this reason the parking demand data for December has been used as the basis for parking recommendations. The existing zoned commercial land within the Town Centre can accommodate retail and commercial development up to 9,000 sqm and an additional 50 - 100 dwellings. Analysis indicates that peak parking demand will increase by between 200 - 450 spaces depending on the rate and quantity of land use growth that occurs. There is little spare capacity in existing parking facilities to accommodate this growth so it is important that the number of parking spaces provided increases at a similar rate.



### Legend

- Unrestricted
- 1/2P
- 1P
- 2P
- No stopping
- x% Peak occupancy - Dec 2012

Figure 39: Peak parking occupancy - 6 December 2012

# Managing Parking

The following objectives and guidelines provide the framework for how car parking is managed as the Town Centre grows:

## Design objectives

- The supply of car parking adequately caters for demands generated by the Town Centre.
- The supply of car parking increases in line with development growth.
- The most convenient parking is available for use by customers and visitors.
- Adequate long term (employee) parking is available within a reasonable walking distance of the Town Centre.
- A shared pool of public parking is used to efficiently cater for demands generated by a variety of land uses.
- Nearby residential areas are not significantly impacted by overflow parking from the Town Centre.
- Car parking areas should operate safely and efficiently for all road users.
- Car parking areas should be consistent with desirable urban design outcomes.
- Vehicular access management should limit impacts to the through traffic function of nearby arterial roads.

## Design guidelines

- Large areas of public car parking central to the Town Centre should be retained.
- Where identified, developments on land currently used as public parking must retain, replace (i.e. basement) or offset these spaces in close proximity to the Town Centre, in addition to any new requirements generated by the development.
- Maintain short term restrictions (for customers / visitors) within the business and mixed use zoned areas of the Town Centre.
- As a premium location for visitor parking, The Terrace should be maintained as high turnover (half hour / 1 hour) parking.
- The number of different parking restrictions should be limited and area parking controls should be maintained.
- Public car parks to incorporate safe, attractive and convenient pedestrian facilities along key desire lines.
- Car parking facilities should be attractive and convenient to use. Relevant considerations include the likelihood of finding a vacant space, efficient vehicular

access, free/inexpensive parking, safe and convenient pedestrian linkages and security.

- Unrestricted parking should be peripheral to the core of the Town Centre with safe walking routes and be carefully located to minimise amenity impacts to residential properties.
- New developments should provide for on-site car parking or as an alternative provide parking in an alternate off site location. Redevelopment of existing sites is encouraged and where it is reasonable to do so, parking provisions may be waived.
- Development car parks should recognise and manage accordingly the separate needs of employee/resident parking versus customer/visitor parking.
- Residential development should include resident parking provisions on-site where reasonable to do so.
- Developments with customer/visitor parking on-site should ensure these spaces remain accessible and attractive to these users. Relevant considerations include visibility from the street, signage, pedestrian connections, lighting and security.
- When provided on private land, public car parks should be managed accordingly. Relevant considerations include maintenance responsibilities, parking restrictions, signage and enforcement.
- The design of new parking facilities should accord with relevant design guidelines including the Planning Scheme and Australian Standard for Parking Facilities (AS2890).



Figure 40: Existing poor pedestrian infrastructure to be improved

# Managing Parking

## Projects

### Main Carpark

The main off-street car park in the Parade has provided operational and safety challenges for Council and the community for many years. The fundamental issue with the operation of the car park is an over supply of vehicle spaces at the expense of pedestrian amenity.

The key challenges for the car park identified are:

- Safety risks associated with vehicles reversing from 90 degree spaces across The Parade;
- Limited and narrow pedestrian pathways;
- Poor visibility for drivers at some pedestrian crossings;
- One-way aisles are confusing for motorists and require

unnecessary circulation movements;

- Trucks unloading in the car parking areas;
- Speed humps and pedestrian crossings are not integrated along The Parade and within the car park.

Low cost and immediate solutions are identified in Figure 41. This solutions result in minimal loss of spaces and can be achieved without expensive kerb realignment.

In the longer term a redesign of the main car park is required. An indicative layout is shown in Figure 42. The complete redesign of the car park would result in a loss of 20 -25 spaces but significantly improved pedestrian amenity. A redesign and off-set of lost spaces should be considered as part of the redevelopment of Hodgson Street and The Parade intersection and long term redevelopment of the key corner sites of the car park.

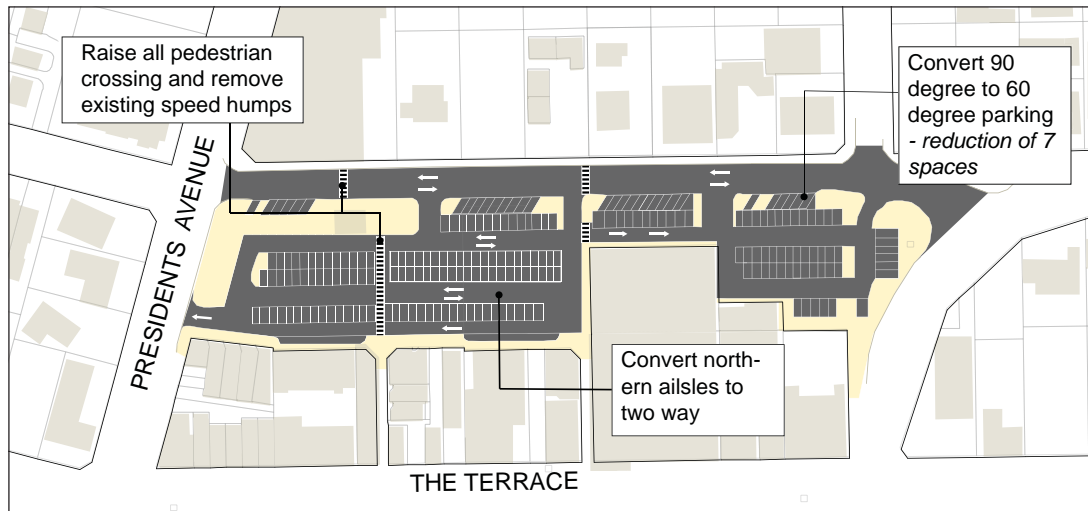


Figure 41: Short term car park upgrades

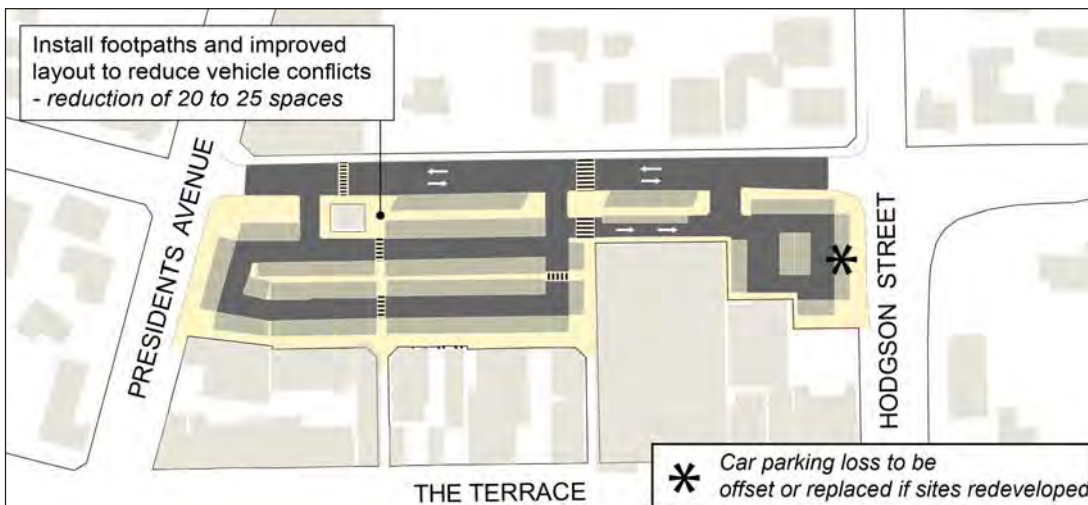


Figure 42: Long term car park upgrades

# Managing Parking

## Additional Parking Supply

As identified in the earlier projects additional parking capacity can be found within the on-street network:

The opportunities are:

- Convert the perimeter of Ocean Grove Park in Presidents Avenue and The Avenue to indented parallel parking thereby allowing the no stopping restriction on the south side of The Avenue. Subject to detailed design this option could result in 10 additional spaces;
- Convert 45 degree angle parking and parallel parking on the north side of The Terrace (west of Hodgson Street) to 60 degree angle car parking (approximately 20 spaces).
- Convert parallel parking on the south side of The Terrace (east of Hodgson St) to 45 degree angle parking.

The Ocean Grove Structure Plan directs that the main Parade off street carpark be relocated underground to provide additional parking supply and improved urban design outcomes.

Through this project Council has investigated the possibility of buying additional land for car parking or constructing a multi-deck or basement car park on the main car park site. One option is that Council collects money (typically \$12,000 per space) through a parking overlay from business owners or developers in lieu of the provision of

car parking spaces on their sites. Based on the number of car parking spaces waived in recent years, high land costs and construction costs in it is highly unlikely that Council would ever raise enough money to fund the construction of decked or basement parking that will cost tens of millions of dollars or purchase land worth multi-millions of dollars for additional carparking.

After consideration of the likely demand for additional parking and the options for additional provision by Council, it is recommended that car parking demand be met in the future as part of new developments in line with the design objectives and guidelines for parking described earlier.

This will include negotiated outcomes with developers or new businesses to improve on-street parking facilities or provide new parking sites. The waiving of parking spaces should only be considered in exceptional circumstances such as the reuse of an existing shop or upstairs office.

## Managing Existing Parking Better

There is also opportunity to use the parking assets that already exist better these are:

- Maintain existing parking time and pricing controls and review parking enforcement schedules;
- Improve directional signage as part of a directional signage strategy for Town Centre; and
- Improve signage to Ocean Grove Plaza parking for increased visibility.



Figure 43: Location of recommended parking changes

# Implementation



## Implementation

This section identifies the actions to be undertaken to implement the policies and recommendations of the UDF.

The table outlines short, medium and long-term implementation milestones to be achieved over a 10+ year period. It is also important to note that different council departments and government agencies will affect certain initiatives, but the general principles outlined in the UDF should be used as the overarching guide.

In addition, implementing the UDF will require involvement from a number of private and public organisations such as the Ocean Grove Traders Association and Ocean Grove Community Association taking ownership of projects and initiatives to improve the Town Centre. Council will work collaboratively with community organisations and businesses to realise the objectives of the UDF.

PLANNING	PRIORITY	RESPONSIBILITY	FUNDING SOURCE
Council adopts the Town Centre UDF	Short term	CoGG	Existing budgets
Prepare a Planning scheme amendment to implement UDF including:	Short term	CoGG	Existing budgets
Apply a Design and Development Overlay to Town Centre to achieve UDF objectives			
Remove existing Design and Development Overlay 16 and Design and Development Overlay 27			
Remove Significant Landscape Overlay 7 from mixed use zone land			
Apply public acquisition overlay to properties in rear lane-way			
Investigate purchase of properties at 97 Dare Street and 37 Hodgson Street and need for public acquisition overlay	Short term	COGG	Council budget bid
Develop a masterplan for the Town Square	Medium term	CoGG	Council budget bid
Install Wifi in The Terrace and Ocean Grove Park	Medium term	CoGG	Council budget bid
Develop implementation plan for squared up intersection - consider a range of issues including land status, land acquisition, infrastructure costing and delivery, market demand and funding opportunities.	Short term	CoGG VicRoads	Existing budgets
Detailed design for Hodgson Street streetscape upgrade			
Develop wayfinding and public art concept for 'Green links'	Short term	CoGG	Council budget bid
Sale process for disposal of surplus Council property	Short term	CoGG	Existing budget bid
Develop a Principle pedestrian network & footpath implementation strategy	Short term	CoGG	Council budget bid
Work with the newly created Tourism Greater Geelong and The Bellarine to promote the Ocean Grove as a unique destination	Short term	Business Association	N/A
Develop an overall branding and signage strategy for Ocean Grove	Medium term	CoGG Business Association Community Association	Council budget bid

[1] Priority: Short term (0-5 years), Medium term (5-10 years), Long term (10+ years)

## Implementation

INFRASTRUCTURE PROJECTS	PRIORITY	RESPONSIBILITY	FUNDING SOURCE
<b>PRESIDENTS AVENUE</b>			
1. Roundabout construction at The Parade Intersection	Short Term	Council	Capital funding bid
2. Roundabout construction at The Terrace Intersection	Short Term	Council	Capital funding bid
3. Streetscaping works - Presidents Ave Green Link - On-road cycle lanes	Short Term	Council	Capital funding bid
4. Streetscaping works - Presidents Ave Green Link - Landscaping	Short Term	Council	Capital funding bid
5. Signalisation of Presidents Ave / Orton St Intersection	Short Term	VicRoads	State funding
6. Streetscaping works between Powell St West and The Avenue - Parking & Shared Path	Medium Term	Council or developer	In-conjunction with development of neighbouring sites by CoGG or developer
7. Streetscaping works at Field St / The Terrace - Shared Path	Medium Term	Council	Special rates and charges scheme and capital funding bid
<b>HODGSON STREET</b>			
8. Squaring up of Hodgson St / The Parade intersection	Short Term	Council / VicRoads	Land development agreement with public and private sector input
9. Streetscaping works - Hodgson St Green Link - Landscaping	Short Term	Council / VicRoads	Capital funding bid
<b>THE TERRACE</b>			
10. Streetscaping works on The Terrace - Landscaping & Parking	Short Term	Council	Capital funding bid
11. Town Square	Long Term	Council	Land development agreement with public and private sector input
12. Completion of The Terrace Rear Lane-way	Long Term	Council	Special rates and charges scheme
<b>THE AVENUE</b>			
13. Streetscaping works - Car parking	Short Term	Council or developer	In-conjunction with development of neighbouring sites by CoGG or developer
14. Streetscaping works - Footpaths & Landscaping	Medium Term	Council	Special rates and charges scheme
15. Landscaping and improvements on The Avenue Pedestrian Link	Short Term	Council	Capital funding bid
<b>THE PARADE</b>			
16. Streetscaping works - Car Parking	Short Term	Council	Capital funding bid
17. Streetscaping works - Pedestrian Crossings	Short Term	Council	Capital funding bid
18. Final Main Car Park Upgrade	Long term	Council or developer	In-conjunction with development of neighbouring sites by CoGG or developer

# Implementation

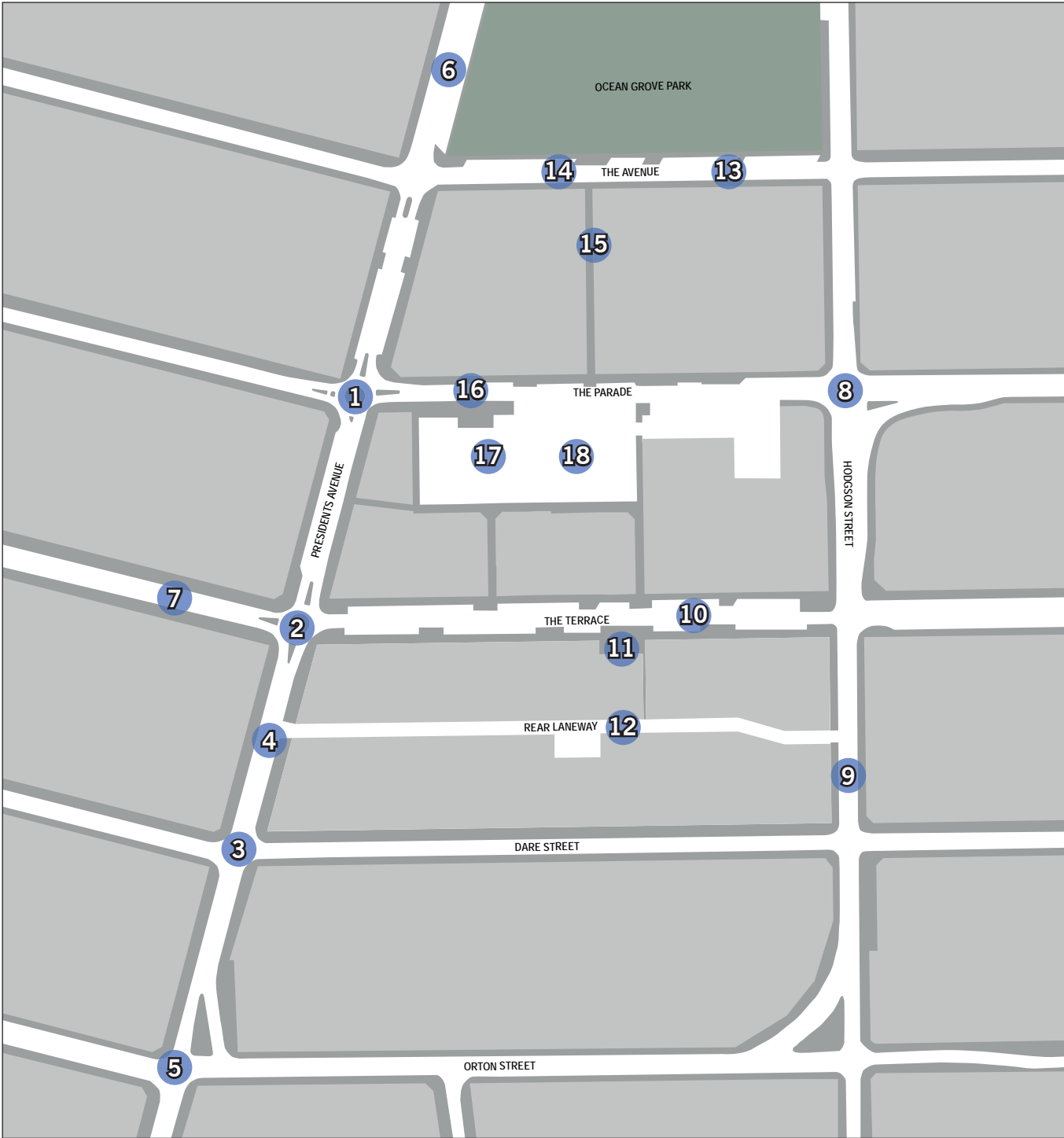


Figure 44: Implementation project plan

# Appendix 1

Ocean Grove Town Centre Urban Design Framework  
Workshop Outcomes - June 2013



# Ocean Grove Town Centre Urban Design Framework

## Workshop outcomes - June 2013

**This bulletin provides information on the outcomes of the Ocean Grove Town Centre Enquiry by Design workshop held on 29th & 30th April.**

Council conducted consultation with residents, business, community associations, sporting clubs, schools and government agencies to start a plan for the future Town Centre.

90 representatives gathered at the Ocean Grove Surf Life Saving Club to uncover the community aspirations for the Town Centre. The two day conference was facilitated by Lynda Jones of groupwork.

Council and the community were assisted by a Design Team with expertise in urban design, retail and activity centre planning and traffic engineering.

Following a full day of discussion, debate, learnings and a few laughs the Design Team worked overnight and throughout the following day to create a possible masterplan for the Town Centre.

Discussion groups considered:

- the aspirations for the future Ocean Grove;
- what was currently good and not so good about the Town Centre; and
- four themes - Traffic movement and connections; Retail and Commercial Activities; Public Space and Streetscapes and Identity.

A full copy of all outputs of the workshop session is available on request.



Participants want Ocean Grove to be seen by others as:

- Unspoilt, Beautiful Environment**
- Friendly, Welcoming and Hospitable**
- Relaxed and Peaceful**
- Vibrant and Attractive**
- Safe**

### What we heard the future Ocean Grove should be:

These objectives are the culmination of the work on day one. They represent the preferred future outcomes for Ocean Grove by 2020, as expressed by the participants:

- Ocean Grove has 'heart and soul' with spaces for the community to gather. It is attractive and vibrant with retail, business, social and cultural activities. It's big beach, ocean and surf character is evident.
- The urban structure encourages people living in quality housing and working in the Town Centre.
- Our Design and Planning Guidelines support developments that advance the special character of Ocean Grove.
- Ocean Grove has a strong connection to and respect for its many environments. Conservation, new plantings and 'green' links of vegetation between key destinations also add to the area's special character.
- Ocean Grove provides a network of roads that don't always give priority to traffic. There is strong pedestrian and cycle connections between the Park, Town Centre and Beach.
- Ocean Grove intersections and crossings are safe.
- Ocean Grove has a well-structured car-parking regime. There's enough parking spaces, access and circulation is easy and it's well managed.
- Ocean Grove has a road network that provides for local traffic and servicing. Through-traffic is slowed as it moves around the town.

# Town Centre Masterplan Concept

On day two most of the group of 90 got back together for a presentation by the Design Team on a **possible masterplan** for the town centre that captures the ideas from the previous day.

The following plans and commentary are the unedited findings of the presentation by the Design Team.

## Economics

From the Design Team, retail planning expert Mike Cullen from Urbacity described the base economics that will effect the future development of the town centre. Key findings were:

- The retail catchment of the Town Centre includes Ocean Grove, Barwon Heads and Point Lonsdale - a current catchment of 22,000 people;
- The Ocean Grove retail catchment will grow by 5,000 people by 2031;
- With the existing and proposed retail stores and supermarkets in Ocean Grove, Drysdale and Leopold there is more than sufficient retail floorspace supply in the catchment to meet future needs;

- The existing Town Centre is under threat from a comprehensive retail-only offer at a new neighbourhood centre on Grubb Rd;
- The growth of Ocean Grove won't change the role of the Town Centre so we need to **improve the quality** of what we've got to retain and attract visitors.
- To do this a range of business and housing options at a range of price points are required.

A potential strategy is to allow office space on the ground floor of new developments on the streets approaching the Town Centre. This approach would increase housing diversity in the Town Centre and free up retail space currently occupied by office uses.

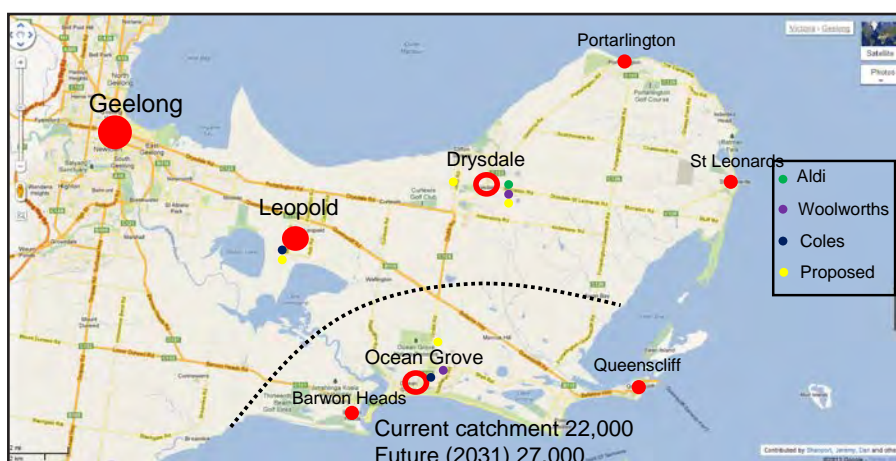


Figure 1: Ocean Grove Town Centre retail catchment and major supermarkets

***“we need new reasons to come to the Town Centre”***  
**- EdD participant**

# Town Centre Masterplan Concept



Figure 2: Potential to grow offices on ground floor with residential above

## Transport

Traffic and car parking generated much of the discussion on day one & day two. Options to address concerns that were raised by the community were considered by the Design Team. Damien Hancox from TTM Consulting considered the possible traffic solutions and provided the following analysis and suggestion of options.

### ***The Terrace – One way reversed***

*Retain the traffic flow as one-way on the The Terrace but reverses traffic to travel east - west.*

#### Advantages

- Supports The Terrace as the Arterial Road
- Removes turning conflict at Terrace/ Presidents Av
- Parking numbers remain unchanged

#### Disadvantages

- CHANGE
- Cost to alter the traffic signals at Terrace/ Hodgson

### ***Parade /Hodgson Intersection***

*'Square up' the intersection of Hodgson Street and The Parade through the installation of traffic signals and removing the sweeping bend in the road.*

#### Advantages

- Pedestrian facilities across Hodgson Street to other parking areas
- Increased kerbside parking on Hodgson Street

#### Disadvantages

- Slight increased travel time along Arterial Road route

# Town Centre Masterplan Concept

## **Arterial Route – The Terrace**

Convert the arterial road from The Parade to The Terrace and provide connection from Tuckfield Street to The Terrace.

### Advantages

- Convenient access to Town Centre (if one-way flow reversed)
- Better on-road facilities for cyclists
- Kerbside parking
- Easier manoeuvring for commercial vehicles
- Through traffic can see the Town Centre

### Disadvantages

- Increased traffic on The Terrace
- Increased traffic on The Avenue to access parking on The Parade

## **Orton St /Presidents Ave**

Install new traffic signals at the intersection

### Advantages

- Pedestrian and cyclist crossing facility between the Beach and Town Centre
- Accommodates all turning movements
- Creates gaps in traffic flow along Orton Street

### Disadvantages

- Increased travel time along Arterial Road route
- Road cyclist facilities along Orton Street
- Increased traffic on Presidents Avenue

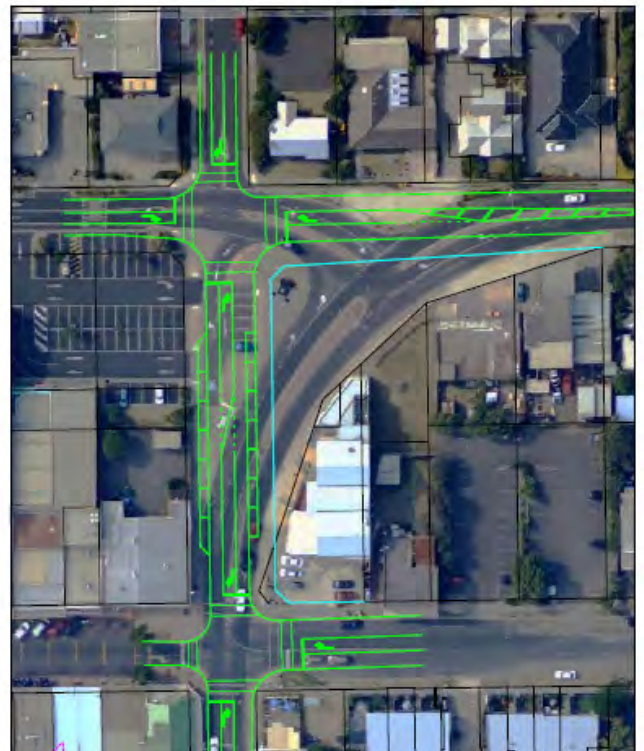


Figure 3: Possible intersection layout at Hodgson Street and The Parade

**“we want a crossroad, not a freeway”  
- EdD participant**

# Town Centre Masterplan Concept

## Urban Design

Experienced architect and urban designer Steve Thorne of Design Urban prepared the overall masterplan concept. The following principles and discussions points guided preparation of the concept masterplan:

- Ocean Grove needs a Heart and Soul – A Meeting Place
- The Terrace is great with strip shopping - not an enclosed shopping centre
- The parking on The Parade is difficult and dangerous
- The plan to improve the Parade/ Hodgson St Intersection has been on the books for 30 years
- There needs to be green links to connect the Town Centre to the Beach
- There needs to be better north-south linkage in the Town Centre
- Ocean Grove Park is highly valued as a gathering and recreation space

The concept masterplan includes a pedestrian link from Ocean Grove Park to a new Town Square in The Terrace to create new north south linkages. The masterplan seeks to expand the Town Centre to create active frontages on The Avenue, Hodgson Street and Presidents Avenue. New retail development on Hodgson Street takes advantage of new traffic lights to 'square up' the intersection of The Parade and Hodgson Street.

The concept of a Town Square and 'green links' of new trees to link the Town Centre and the beach were enthusiastically received by the community. There was also very strong support for the installation of traffic signals by VicRoads soon at the corner of Presidents Avenue and Orton Street to improve road and pedestrian safety.

The following plans are the **unedited concept masterplan** that was created the Enquiry by Design workshop.



Figure 4: Active frontages to be encouraged on The Terrace, Hodgson Street, Presidents Avenue and The Avenue

***“Ocean Grove is the best holiday place to live”***  
- EdD group

# Green Links to the Esplanade

*Linking the Park to the Town Centre to the Beach*



# Making Parking Safer

*Realigning Parking so that The Parade is safer*



# Making Ocean Grove easier for Pedestrians

*Creating a Network of Safe Footpaths*

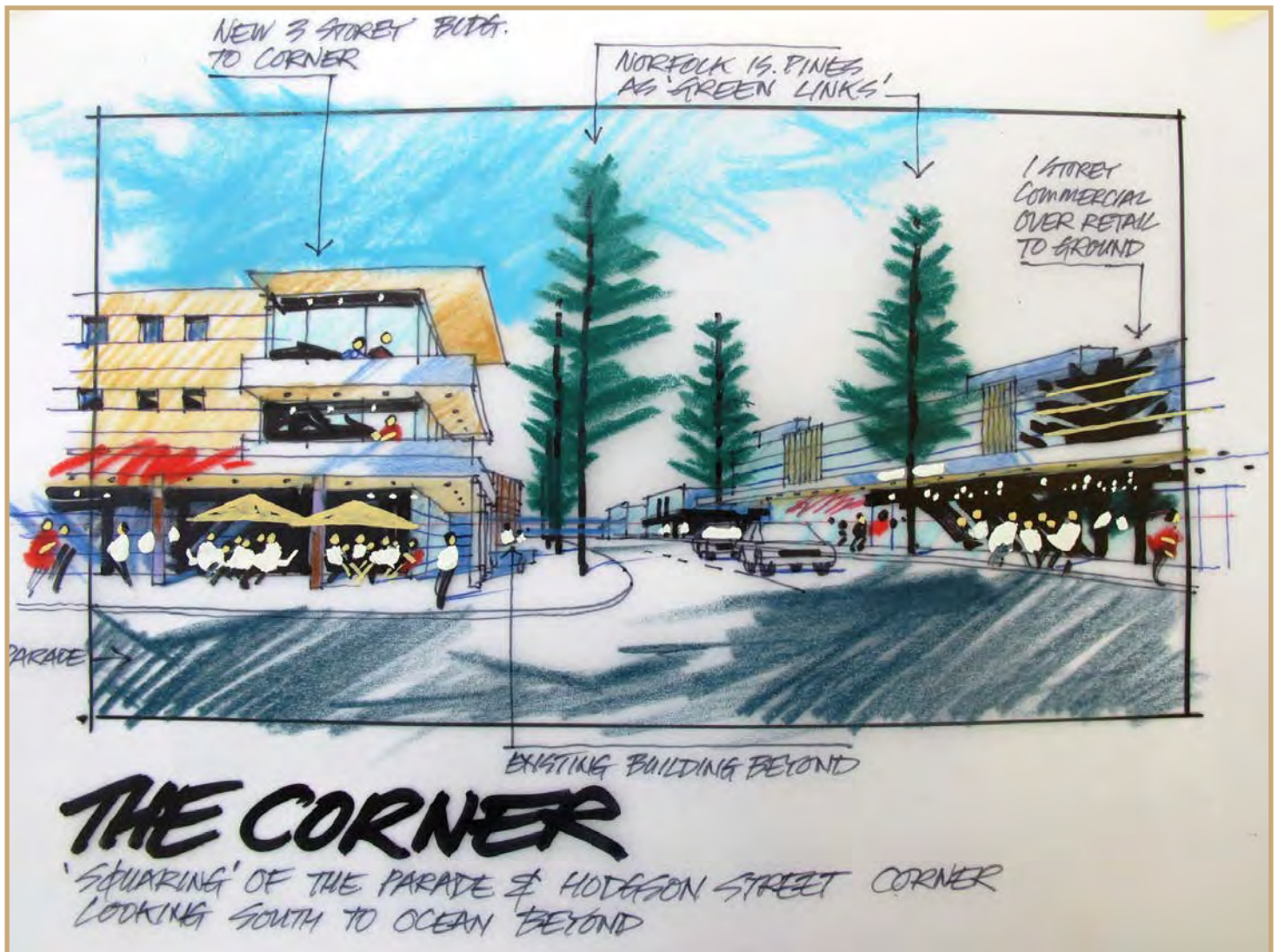


# Improvements to the Intersection of The Parade and Hodgson Street





Existing site photo



# THE CORNER

'SQUARING' OF THE PARADE & HODGSON STREET CORNER  
 LOOKING SOUTH TO OCEAN BEYOND

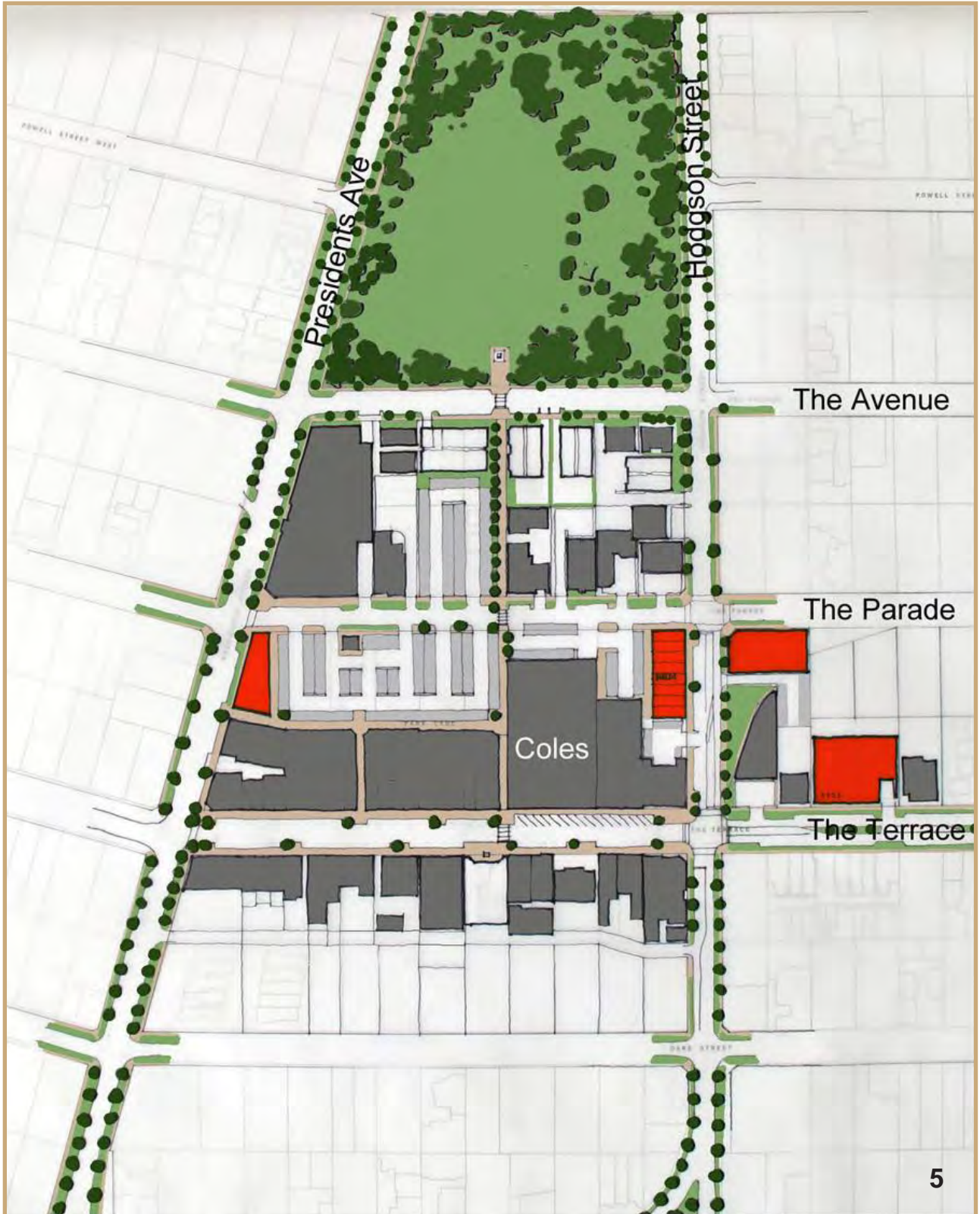
# Growing the Town Centre

*A Potential New Retail Outlet*



# A New Hall or New Shops

To improve President Avenue





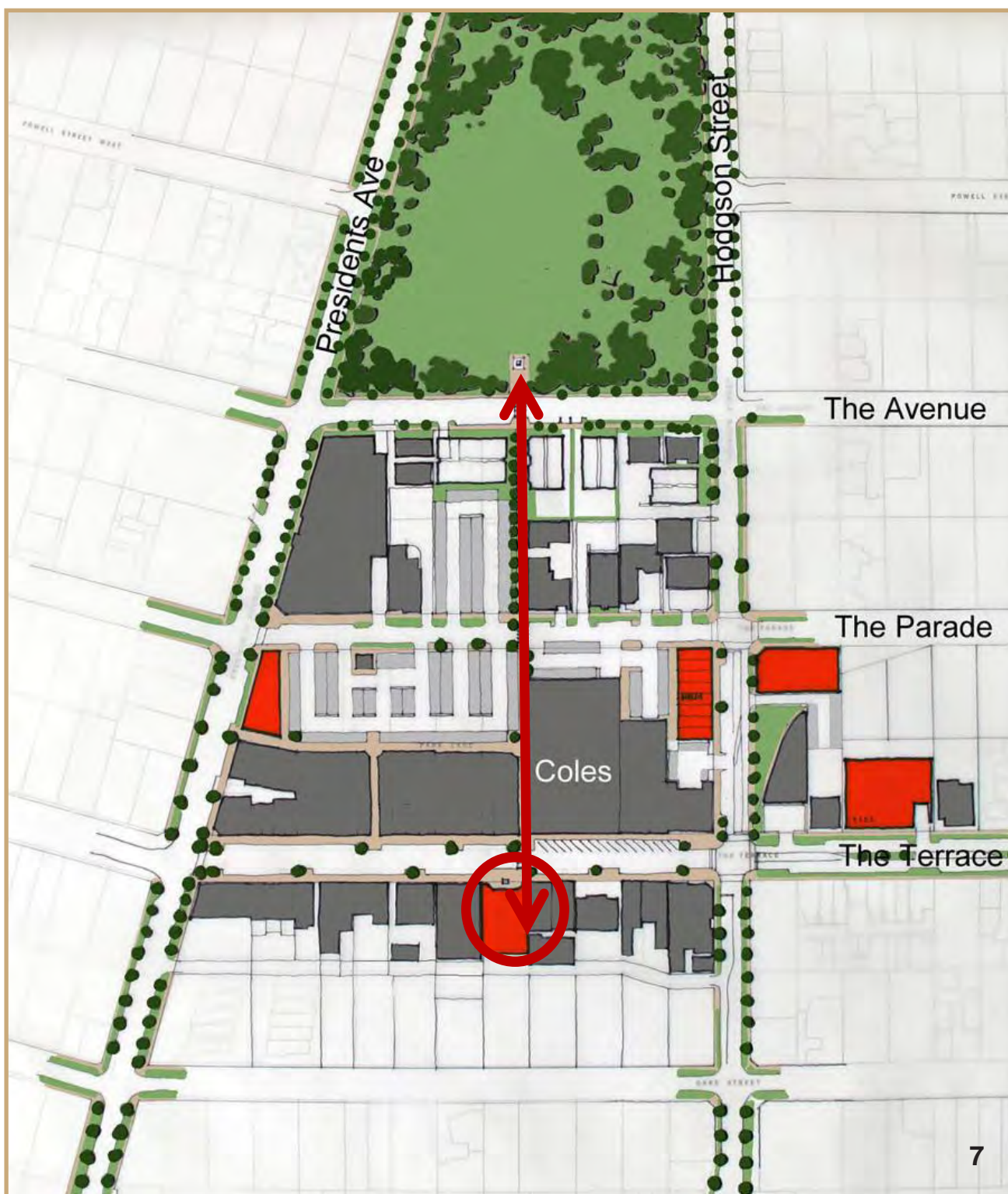
Existing site photo

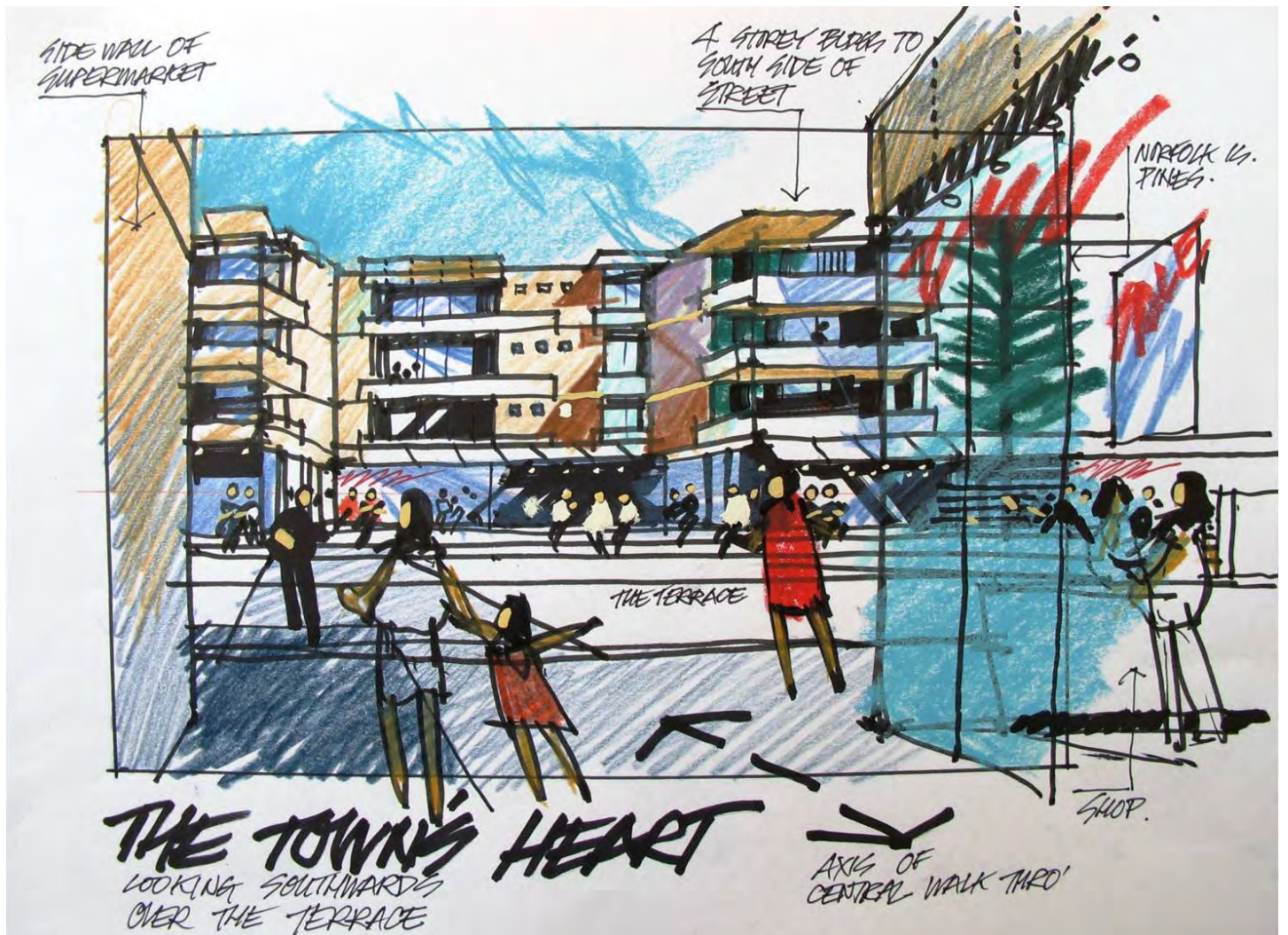


## Growing Towards Ocean Grove Park



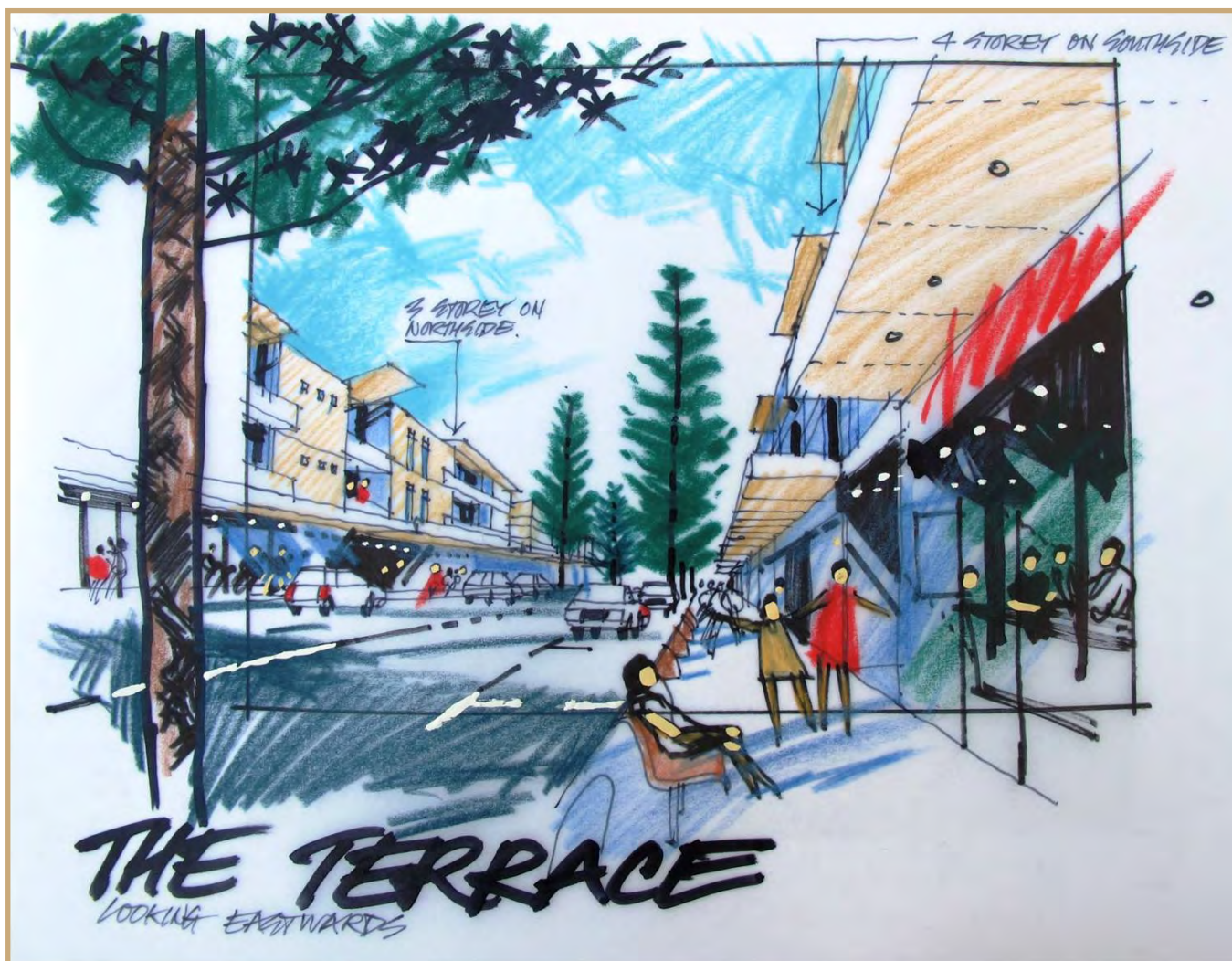
## A New Meeting Space on the Sunny Side of The Terrace in the Heart of Town







Existing site photo



# Putting it All Together

*A Vision for Ocean Grove Town Centre*



## Next steps

The Enquiry by Design is not the forum to create the final plan it is just the beginning of testing ideas and concepts. Council will now take the findings of the consultation to further refine the concept plan with more detail and analysis.

Council will undertake further consultations and negotiations with affected landowners and authorities.

Council will then come back to the community with a draft UDF for further consultation before the end of the year.

Following the further consultation Council will consider the UDF and a planning scheme amendment to im-

plement the strategy will be prepared in 2014.

This is a long term project for the Town Centre that will guide change over the next 20 years.

Concurrently with the planning scheme amendment Council will commence development of concepts for development sites, prepare detailed designs for public realm works and work with VicRoads on intersection improvements with the view to securing budget allocations for the works in the following years.



# Appendix 2

## GLOSSARY OF TERMS



**Active frontage**

Refers to street frontages where there is an active visual engagement between those in the street and those on the ground floors of buildings. This quality is assisted where the front facade of buildings, including the main entrance, faces and open towards the street.

*(DTPLI Activity Centre Guidelines, page 46)*

**Arterial roads**

Arterial roads are the principal routes for the movement of goods and people within an area's road network. Arterial Roads have traditionally been further divided into primary and secondary arterials. Secondary arterials supplement the primary arterial roads by providing for through-traffic movement to a determined carrying limit that is sensitive to roadway characteristics and abutting land uses.

*(VicRoads traffic engineering manual, vol 1)*

**Green link**

Pedestrian and cycle connections between the beach, Town Centre and Ocean Grove Park reinforced by landscaping and tree planting themes.

**Mixed use development**

Good mixed use development involves the fine-grained mixing of compatible land uses in a balanced mix. Physically, it includes both vertical and horizontal mixes of use. No single use should dominate other uses, and residential land use should generally not exceed 60% of the land use.

*(DTPLI Activity Centre Guidelines, page 46)*

**Principle pedestrian network**

A designated network of routes in a given area which support walking trips into and around key destinations such as activity centres, schools and transport nodes.

**Shared carparking**

Two or more uses share carparking areas and access points, rather than many single carpark areas. Day and night uses, weekday and weekend uses also share carparks.

**Signature building**

New buildings of a high design quality that consider the prominence and importance of the site as a key entry to the Town Centre.

**Special rates and charges scheme**

The Local Government Act (1989) provides Council with the ability to introduce a Special Rate or Charge Scheme to seek contributions from property owners towards infrastructure projects such as roads, footpaths, streetscapes and drainage improvements. A special rate and charge scheme may also be levied on commercial properties within a shopping area for the purposes of marketing and promotion of the centre.

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